

# FORMER PRIAULX GARAGE



## DEVELOPMENT BRIEF

**July 2016**

# Contents

## Introduction

- 1.1 Purpose  
Policy Context

## Site & Context Analysis

- 2.1 **The Site**  
General Description Of Site  
Topography Of Site  
Existing Boundaries
- 2.2 **The Site (continued)**  
Utilities On/Off Site  
Nearby Amenities
- 2.3 **The Site (continued)**  
Existing Landscape and Biodiversity  
Built Environment/Pattern
- 2.4 **Context**  
Materials  
Neighbours  
Density
- 2.5 **Context (continued)**  
Location  
Movement
- 2.6 **Wider Context - Photographs**

## Development Guidelines

- 3.1 Potential Uses
- 3.2 Density  
Building Scale & Height  
Frontage To Les Oberlands & Ville au Roi  
Potential Effect On Neighbours  
Landscape, Biodiversity & Open Spaces  
Site Conditions  
A Place With A Distinctive & Positive Character  
Amenity Of Prospective Residents
- 3.3 Recycling/Waste Strategy  
Foul & Surface Water Drainage  
Access & Parking  
Archaeology  
Potential Of Demolition Of Existing Buildings  
Lifetime Homes Standards

## Appendices

- Appendix A** Key UAP Policies
- Appendix B** Lifetime Homes Standards
- Appendix C** Topographical Survey
- Appendix D** Mains Drainage Plan



## Introduction

### Purpose

The purpose of this Development Brief is to provide planning guidance for the residential development of this site, considering its optimum use in providing accommodation for key workers and potential social housing. The proximity of the site to the Princess Elizabeth Hospital could provide valuable accommodation for staff (key workers) at the hospital.

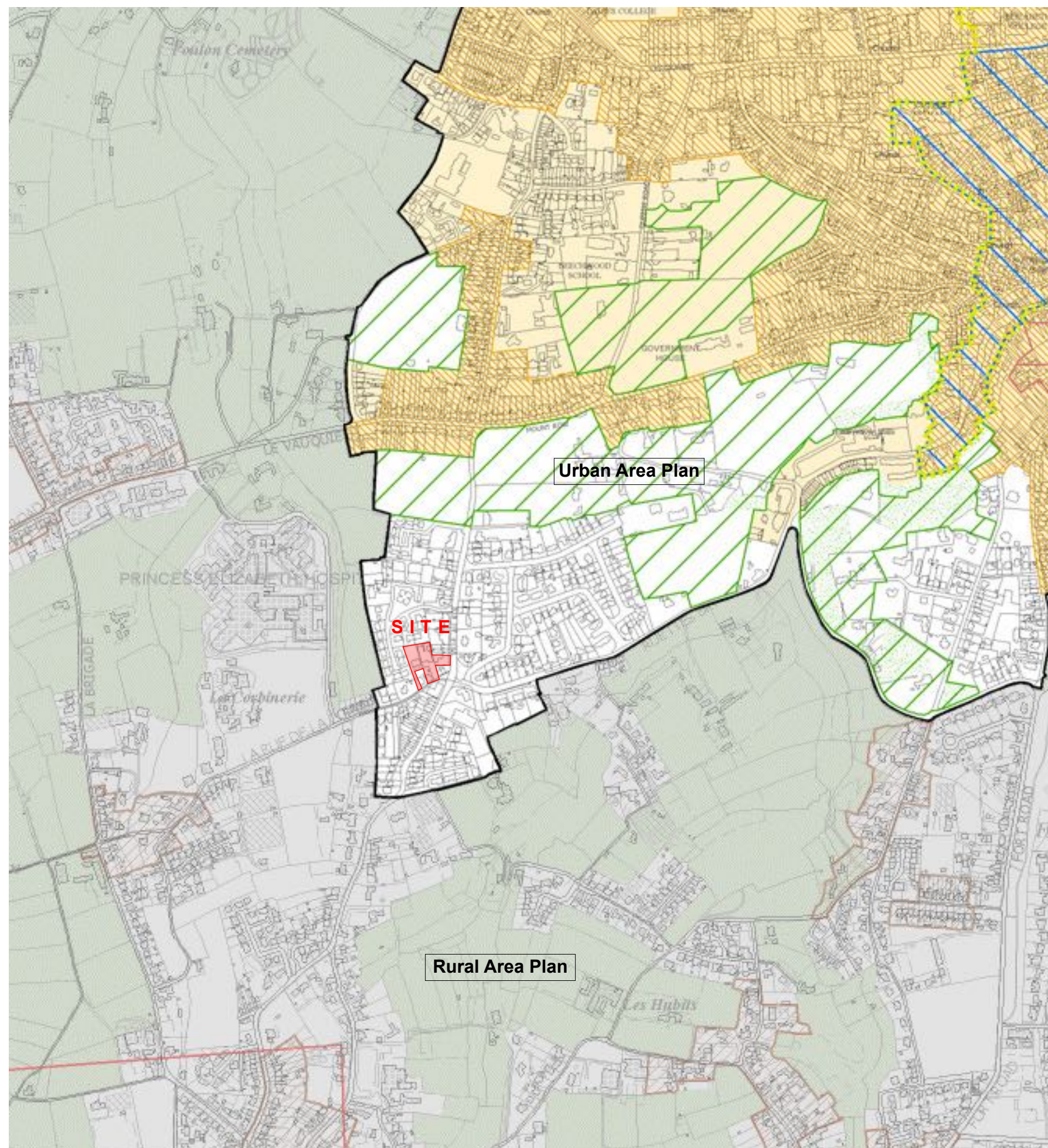
The 2007 States Report, *An Integrated Corporate Strategy to Meet the Island's Needs for "Key Workers"* (Billet D'Etat XI), defines key workers as employees who deliver a frontline public service where the recruitment and retention of staff has proven to be difficult, they are in possession of specialist/professional skills, qualifications or experience and they are in receipt of incomes that do not enable them to afford to buy or rent in the general housing market.

This document considers the environment of the site and the impact any development will have on the character of the area and its users. It provides guidance as to how the policies of the Urban Area Plan (UAP) will be applied to produce an appropriate and beneficial form of development, setting out broad design principles which will allow a flexible approach to design development.

The planning policy framework of the Urban Area Plan provides the opportunity for other potential uses of the site. If an alternative use were to be proposed a separate Development Brief may be required.

### Policy Context

The site is previously developed land outside the Settlement Areas within the Urban Area Plan Review No.1, July 2002. The Urban Area Plan policies generally support the redevelopment of the site for a range of potential uses including residential accommodation. A detailed list of relevant policies for a residential development and interpretation is set out in Appendix A.





## Site & Context Analysis

# 2.1

### The Site

#### General Description Of Site

The site fronts onto the north side of Les Oberlands, close to and west of the major road junction and roundabout of Ville au Roi, Ruettes Braye, Les Friteaux and Les Oberlands.

The site area is approximately 0.3 hectares (1.95 vergées) and has a 40.5m frontage (east to west) with a site depth of approximately 70m (south to north). A section of the site, formerly The Late Shopper, fronts Ville au Roi and has a frontage of 18m and a site depth of 31.5m.

A dwelling, Boddington, is to the south-west of the site. Its inclusion would be subject to the agreement of the owners and purchase of the land by any potential developer. For the purposes of this Development Brief Boddington will not be considered to be part of the site. However, proposals will be expected to allow for potential future development of the site in an integrated way and to provide reasonable safeguards for its privacy and amenity should it remain.

The site hosts a number of buildings identified on the site plan adjacent.

#### Topography Of Site

The site slopes downwards gradually from south to north. The level change is approximately 1.2m from the site access points on Les Oberlands to the rear (north) boundary of the site. Land associated with properties adjacent to the northern and western boundaries are generally lower than the site (approximately 500mm to 700mm). A topographical site survey is attached in Appendix C.

#### Existing Boundaries

North - 1.8m high fence to western end, high blockwork or granite walls with some planting elsewhere.

East - High blockwork or granite walls with some planting

South - Low walls facing Les Oberlands. The access to the former garage is set back with a high chain link fence and gate.

West - Low granite wall with hedging and trees behind.





## Site & Context Analysis

### The Site (continued)

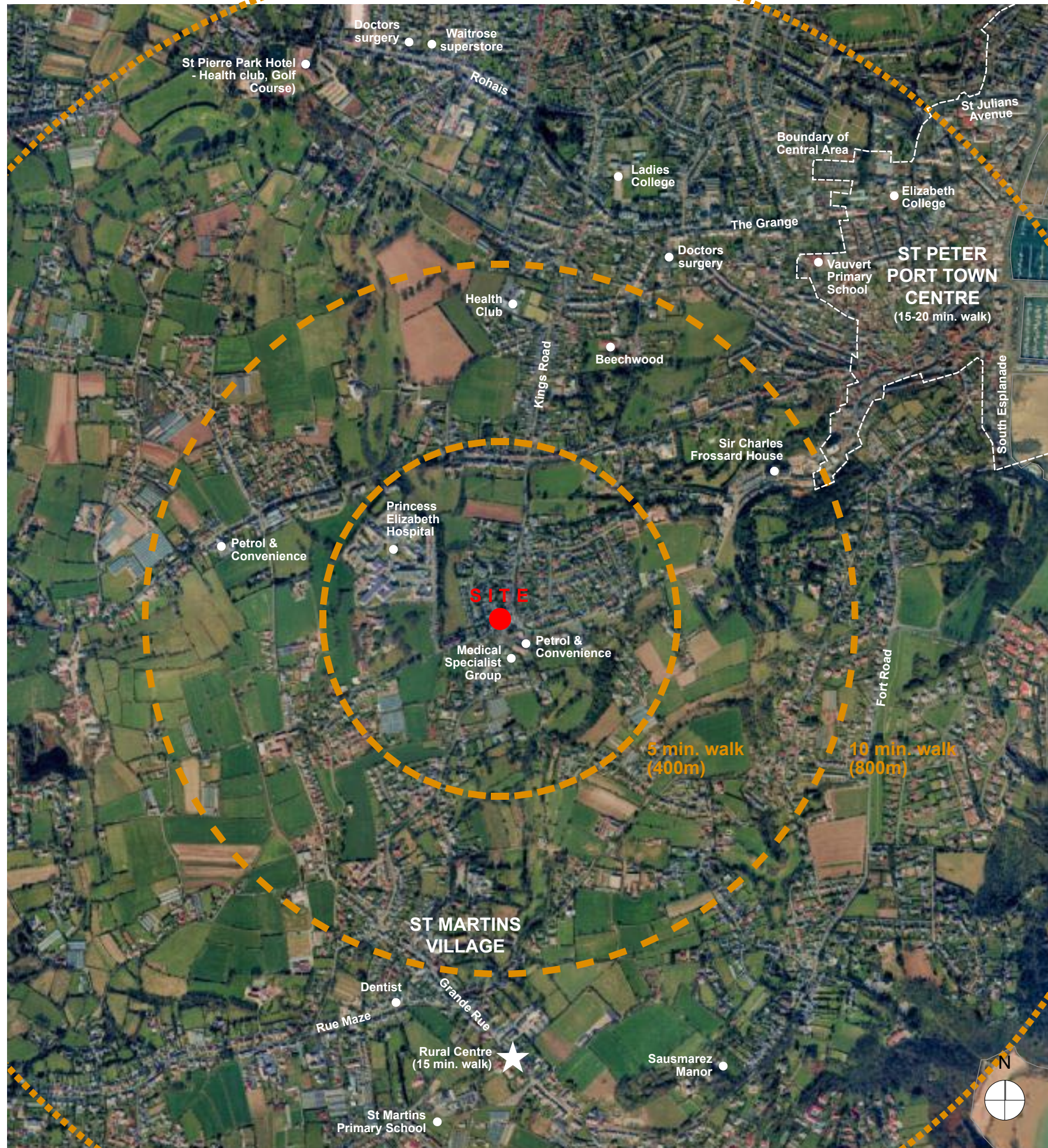
#### Utilities On/Off Site

As an urban site all the major utilities - water, electricity, gas, mains drainage, telecoms - are available. Guernsey Water have confirmed that the mains water infrastructure is likely to be sufficient, see Appendix D for plan of mains drainage layout and connections.

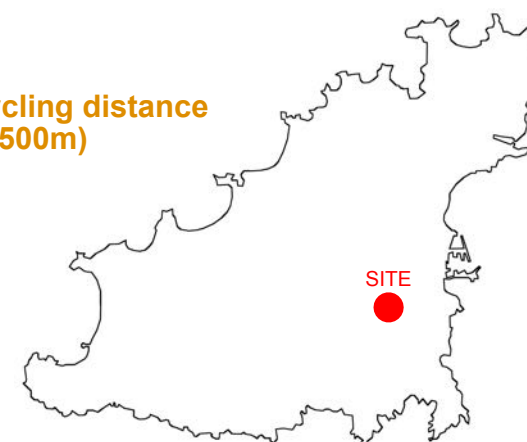
#### Nearby Amenities

The site lies within short walking distance of the town Central Area and 15-20 minute walk of the St Peter Port Town centre and its shops, offices and amenities. St Martins Village Rural Centre including shops, offices and amenities is also within a 15 minute walk. The site is well located for schools, medical care, leisure facilities, and convenience stores.

The site would be ideal for key worker accommodation due to its proximity to the Princess Elizabeth Hospital (5 minute walk), Medical Specialist Group (1 minute walk) and Sir Charles Frossard House (9 minute walk).



Cycling distance  
(1,500m)





## Site & Context Analysis

### The Site (continued)

#### Existing Landscaping & Biodiversity

The external areas which are part of the former Prialux Garage are mainly hard landscaped with tarmac or concrete surfaces. There are areas to the rear (north) of the site which have become overgrown with bracken but would not be considered to represent any significant planted area of habitat. The remaining 'green' spaces of the site, namely the gardens of Beauville, Grassmere and the garden space behind the former Late Shopper, are characterised by domestic lawns and hedges, again, not providing any significant areas of 'natural' habitat. The site does not contain any significant trees.

#### Built Environment/Pattern

The built environment surrounding the site is generally domestic in scale with single, two and three storey buildings adjacent. The site forms part of the settlement pattern around a major junction serving as an important gateway to St Peter Port. The road frontages of buildings on the site and in the immediate vicinity are generally close to the road or abut the pavement.






Regular, traditional patterns of development predominate to the west side of Ville au Roi. The strong street alignment of tightly knit, ribbon development is occasionally penetrated by access roads serving cul-de-sac estates to the rear (west).

Ville au Roi's tree lined pavement (east) and the tree lined streets of the residential clos to the east of Ville au Roi (Avenue Germain etc) make a notable contribution to the character and appearance of the area.

To the east of Ville au Roi, large, detached and semi-detached houses on substantial plots front onto the road while to the rear (east) of those houses lies the suburban residential development on Avenues Germain and Vivier.

The positive street alignment to the west of Ville au Roi and Les Frieteaux comprises two, two and a half and three storey buildings. The main characteristic of the two street scenes is a regular, Georgian style, vertical-rhythm pattern of fenestration.

The pattern of the built environment along Les Oberlands is of detached and semi-detached dwellings with small front gardens and parking directly adjacent to the road. There is no distinct design style to the properties, the buildings having been constructed and developed over a number of decades.

-  Landscape buffer - Area of mature trees or hedges
-  Possible interface, privacy and overshadowing conflict
-  Possible interface, privacy and overshadowing conflict (should Boddington not form part of the development site)
-  Buildings contributing to street scene in vicinity of site
-  Buildings contributing to a positive street scene/alignment in vicinity of site





## Site & Context Analysis

### Context

#### Materials

Generally materials comprise slate, pan tiles or plain terra-cotta roofs, plain rendered walls with some examples of raised plaster features on older properties. There is also limited use of granite in the area.

#### Neighbours

To the west and north of the site, two storey dwellings in Clos du Roi come within 5 metres of the site. Those dwellings have relatively limited private gardens directly abutting the site boundaries.

The east boundary of the site is adjacent to amenity and parking areas serving Kings Court and adjacent residential properties to the north east, and to service yards associated with the existing adjacent shop.

The site frontage (south) faces the rear of a single storey structure currently used as a flooring showroom on Les Frieteaux.

#### Density

High density development is situated mainly along road frontages to the east of the site, notably, Kings Court, Amballa and Manoir de Preaux.

Medium density development exists to the west and north of the site (Clos du Roi). To the east of Ville au Roi and along Les Oberlands to the west, the density is generally lower.



The entrance to the former Priaulx Garage site on the south boundary of the site. The gable of Amballa can be seen on the right and the boundary wall of Grassmere on the left.



Parking area to the rear of Beauville. Boundary formed by fencing. Potential for overlooking into and out of the site will be a consideration.



Former Late Shopper buildings, now vacant. Some areas in state of disrepair though generally appears in sound condition.



The alternative access to the site, currently serving Beauville. The low granite wall and mature trees and hedging can be seen along the length of the driveway.



Grassmere dwelling and associated garage and boundary walls. Dwelling is vacant and in a state of disrepair though generally appears in sound condition.



New developments of Amballa and Manoir de Preaux forming the corner with Ville au Roi. The development replicates the buildings that previously occupied the site.



Context (continued)

Location

The site is well located for public transport (see diagram opposite), other facilities and services. It is also within walking distance of the Princess Elizabeth Hospital and Sir Charles Frossard House, and, as such, is well located for key workers.

Movement

Pavements exist on both sides of Ville au Roi and Ruelle Braye, and west side only in Les Frieteaux. A pavement turns into Les Oberlands from Ville au Roi, extending across the front of Amballa, after which there is no pavement in Les Oberlands.

Les Oberlands is classified as a Local Circulation route and traffic management issues have been identified due to a combination of factors including the lack of a pavement, traffic flows at peak times and the limited width of the road in places together with it being on bus routes. Two way traffic along Les Oberlands is restricted in places for large vehicles with the width of the road falling below the minimum standard of 5.5m.



- ..... Route 71 Town Terminus - Airport - Town Terminus
- ..... Route 81 Town Terminus - Jerbourg - Town Terminus
- > Traffic Priority Route
- Pedestrian pavement





1. View of the street scene facing Les Oberlands (looking north-east) with the new development making the corner with Ville au Roi in the background.



2. Ville au Roi street scene (looking north) characterised by terraced elevations on the side of the site and individual dwellings set back from the road on the opposite side.



3. View from the car park to the east of the site showing the former Late Shopper buildings and Grassmere in context with the new development and surrounding buildings.



4. View of the buildings opposite and continuing up the west side of Les Frieteaux, including a flooring showroom directly opposite the site and Victorian terrace beyond.



5. View looking from Les Oberlands towards the junction with Ville au Roi showing the potential visibility from the Beauville entrance.

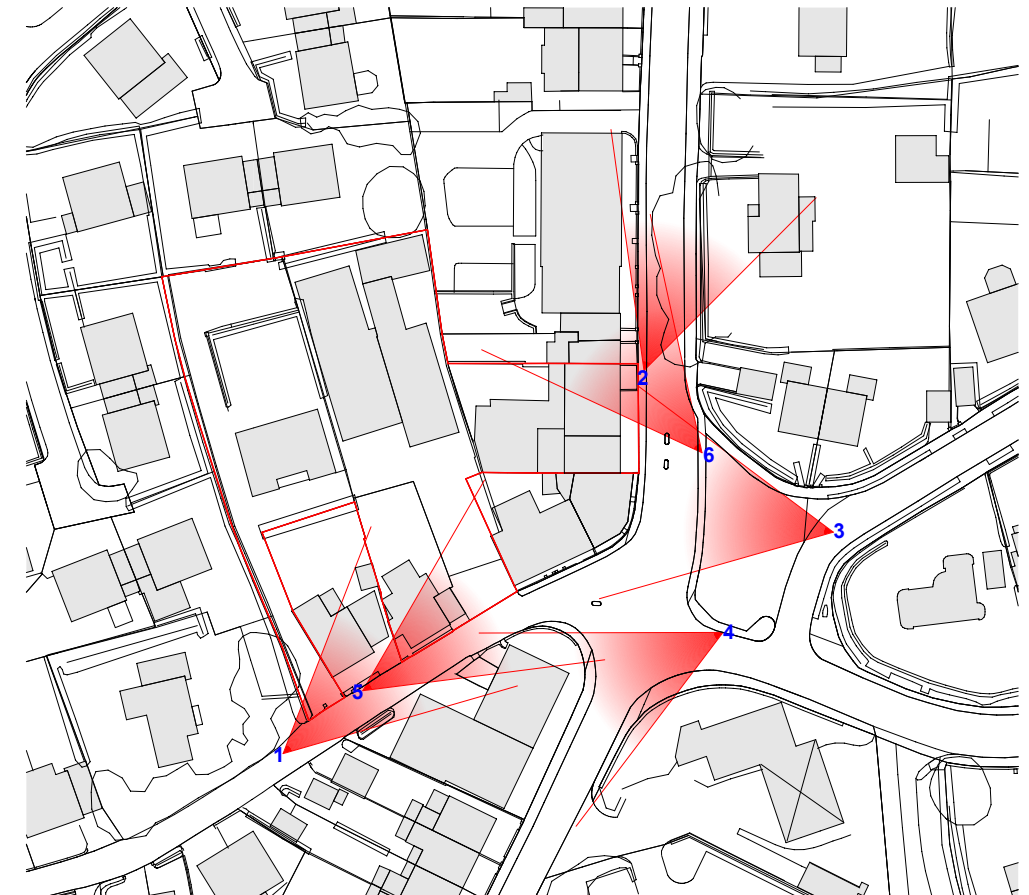


6. View of the west street scene facing Ville au Roi. The former Late Shopper buildings are on the left with the newer Kings Court development further along.

## Site & Context Analysis

# 2.6

### Wider Context - Photographs



Map indicating positions of photographs



# Development Guidelines

## 3.1

### Potential Uses

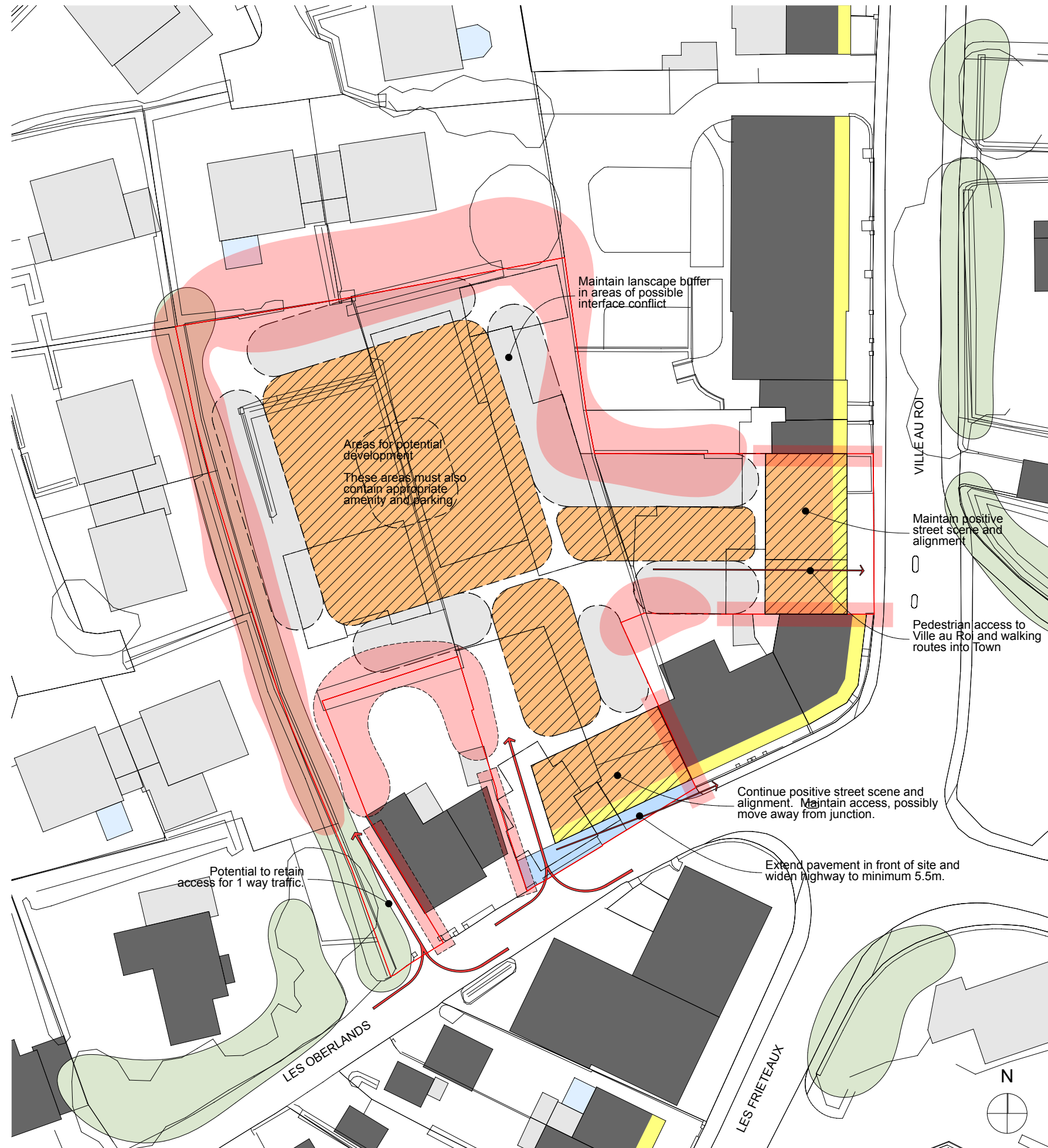
The site falls outside the Settlement Areas within the Urban Area Plan Review No. 1, July 2002. In principle a number of land uses would be acceptable. A comprehensive development is needed to maximise the opportunities of the site. This will incorporate the former garage site and some or all of the neighbouring properties with potential for redevelopment subject to availability. The main part of the site, the former garage and car sales, is recognised as an industrial use. UAP Policy EMP9 requires proposals for a change of use of the garage site to demonstrate that the site is no longer suitable for an industrial use. Given the standard of the buildings, the poor access and the characteristics of the site in close proximity to residential development it is accepted that the garage premises are no longer suitable in land use terms to continued industrial use and such use has been dormant for an extended period. As such UAP Policies allow for a range of potential new uses for the site, subject to satisfying the relevant policies.











Policy HO2 provides a policy gateway for residential development (as the site is outside Settlement Areas on previously developed land) provided the site is suitable in terms of existing characteristics of the site and its relationship with the surrounding area and is acceptable in terms of design, density and amenity - issues addressed in this Brief. Policy CO1 makes provision for infill development on previously developed land within an existing group of buildings. Given the characteristics of the site and the location it would appear to be most suitable for residential development. The former Late Shopper building could offer the potential for conversion/subdivision for residential purpose, subject to achieving satisfactory standards for residential amenity, if it is considered worthy of retention (Policy HO4).

Residential development could take the form of a range of types and tenures to meet identified housing needs. In addition to private market, social and intermediate housing, the size and location of the site, including its close proximity to the Princess Elizabeth Hospital renders it suitable for the development of sheltered or specialised housing, including key worker, subject to normal planning considerations (Policy HO13). Policy HO11 requires any residential scheme to cater for a range of housing needs with the majority of a size, form and type suitable for smaller households (no more than two bedrooms - i.e. 4 habitable rooms or less).

There is also potential for community service facilities which could complement residential development which has a care element (Policy SCR1).

See Appendix A for a comprehensive list of key Urban Area Plan policies.



-  Landscape buffer - Area of mature trees or hedges
-  Possible interface, privacy and overshadowing conflict
-  Possible interface, privacy and overshadowing conflict (should Boddington not form part of the development site)
-  Buildings contributing to street scene in vicinity of site
-  Existing site access to be re-used. Potential vehicular entrances and exits
-  Buildings contributing to a positive street scene/alignment in vicinity of site
-  Potential new development contributing to a positive street scene/alignment
-  Areas for potential development
-  New Pavement and widen highway to 5.5m
-  Buffer Zone



# Development Guidelines

## Density

There is an opportunity for high density development along the street frontages with lower density development to the rear within the site. This will be highly dependent upon the design response to other criteria. For example, satisfactory and appropriate architectural design and scale, potential neighbour conflict issues and provision of suitable amenity space for residents. Based on a mixture of two and three storey buildings there may be an opportunity to provide a high density residential development of between 60-75 dwellings per hectare, which will provide a yield of between 18-23 dwellings for the site. However, it is important to note that this density and yield are indicative. The site may be able to yield more, or less dwellings, subject to meeting all the development and design principles, including the provision of associated car parking and amenity space and depending on the type of housing proposed.

## Building Scale & Height

The scale of buildings is a combination of their width, depth and height. The scale of proposed buildings should reflect the scale of other residential buildings in the vicinity. The scale of the development could increase towards the Ville au Roi junction to tie in with the existing development of Amballa and Manoir de Preaux. The remainder of the development will generally be expected to reduce in height towards the west and north of the site and be no higher than 2 storeys (excluding any roof) in height, based on a maximum storey height of 2.9 metres.

## Frontage To Les Oberlands & Ville Au Roi

There is an opportunity to create a positive street frontage to Les Oberlands and Ville au Roi, continuing the existing alignment. The new development of Kings Court has successfully reinforced the character of the Ville au Roi frontage. The existing frontage may be extended and reinforced although careful consideration should be given to the relationship with neighbouring properties. The height and massing of the existing developments of Amballa, Manoir de Preaux and Kings Court could be reflected in part along the street frontages. There may be scope for a modern interpretation that respects the setting and surrounding of the built environment.

## Potential Effect On Neighbours

Any development needs to respect neighbouring residential properties in terms of potential interface between windows, dominance and overlooking of private amenity spaces, overshadowing, emissions, noise and disturbance as required by Policy GEN12. Several areas of potential interface, privacy and overshadowing conflict have been identified as indicated on the plan on the previous page. Careful consideration will have to be given to the proximity of both the built elements of the development and areas for circulation and parking. Although the scale of development would not usually require a Construction Environmental Management Plan, due to the confined nature of the site and proximity to neighbouring residents, proposals should include details of how the construction process would be managed, in particular with regard to dust, noise, odour and construction traffic.

## Landscape, Biodiversity & Open Spaces

There is an opportunity to reinforce the landscape character of the area, particularly at the north and north-east of the site. Proposals should incorporate a detailed landscaping scheme and maintenance programme which retains and augments boundary planting and seeks to enhance both the public realm and the setting of the development as required by policy GEN3.

A large proportion of the site is currently hard surfaced. Development should seek to provide 'green' spaces as a key element of the design and layout. As this is a fully enclosed urban site a substantial 'landscape buffer' may not be appropriate, especially given the previous development on, and brownfield nature of, the site. Any development should, however, make provision for landscaped areas within the development either as communal or private amenity space, or 'green' areas as part of the overall site layout. This will be particularly important where the interior of the site may be visible from the adjacent highways or outside the site.

## Site Conditions

Given the previous use of part of the site as a garage, the potential for contaminated land would need to be investigated. An initial desk top study of the site would be required in accordance with British Standard BS 10175. It is also recommended that some site investigations are undertaken under BS 5930:2015. Further investigations and remediation works may be required following the results of these studies. Guidance is available for the redevelopment of potentially contaminated sites for residential development.

## A Place With A Distinctive & Positive Character

The development should have a character which draws from the architectural characteristic, materials and colour and the residential buildings in the surrounding area. In order to achieve this principle, it is important to consider the development (the streets, communal spaces, gardens and the buildings) as a whole.

## Amenity Of Prospective Residents

UAP Policies GEN9, HO2 and HO10 encourage the efficient use of land, but not at the expense of the living environment for prospective residents. Annex 3 of the UAP provides guidelines on amenity for prospective residents. In addition, consideration will be given to the Building Research Establishment Report - Site Layout Planning for Daylight and Sunlight, A Guide to Good Practice, Second Edition, P Littlefair (2011).



## Development Guidelines

### Recycling/Waste Strategy

Any development should make adequate provision for recycling and general waste facilities within each unit and communally on the site for purposes of collection.

Additionally, the design and construction method shall be such that waste associated with the development process can be minimised, including provision for re-use of materials on or off site (e.g. use of masonry from demolished buildings as hardcore) and the method of dealing with residual waste. In order for the development to be sustainable (Policy GEN1) and to help support the States Waste Strategy Waste Minimisation Plan (2011), where demolition and particularly where substantial excavation is proposed, any proposal shall address any waste management issues in relation to disposal of demolished and/or excavated material.

It should be demonstrated that the development will be beneficial to the community and sustainable in terms of its location, design and use of existing resources, and its impact on the environment.

### Foul & Surface Water Drainage

Details of foul water drains within the public highway have been provided by Guernsey Water and are contained within Appendix D. No surface water drainage is to be discharged into the foul water drain regardless of any existing situation. All surface water will be expected to be dealt with on site and the development should incorporate sustainable urban drainage systems (Policy WWM5). If it can be demonstrated that this cannot be achieved then consideration will be given for any exceedance to be attenuated and then discharged into the existing surface water drainage system located within the public highway.

### Access & Parking

A Traffic Impact Assessment may be required if the development results in the creation of 25 or more residential units, see Annex 6 of the UAP.

Relocation of the main former garage access further west to enable the continuation and enhancement of the street frontage could be considered provided that the visibility splays, layouts and design of the access meets Part P of the Building Regulations. The secondary access to Beauville may be utilised for one way traffic although appropriate visibility splays would be required and consideration will be given to the impact on neighbouring properties.

The development should seek to extend the pavement along Les Oberlands site frontage (minimum width of 1.2m) and increase the width of the road along Les Oberlands site frontage (minimum width of 5.5m) to enhance the pedestrian environment and enable two way traffic for large vehicles. Consideration should also be given to the creation of a pedestrian access onto Ville au Roi.

With regard to parking and circulation these would preferably be avoided directly adjacent to the north and west boundaries of the site. The guidance on parking standards are set out in Annex 2 of the UAP. Any development should seek to meet the parking standards. However, Annex 2 of the UAP states that the parking standards are not inflexible and variations will be allowed depending on the individual characteristics of the site including; the built environment, on street parking capacity and proximity to public car parks, access and amenity implications for other residents, highway safety, the type of development proposed, accessibility to the Central Area by foot or bicycle and the level of public transport provision.

Care must be given to any on street/surface parking spaces, so that external spaces are positive for all users rather than being dominated by areas of hard landscaping. Parking should, wherever possible, be on street (within the site) and not allocated. However, given the amount and type of development anticipated (see page 3.2 - Density) it is unlikely that all parking can be accommodated on street. Off street options such as rear parking courts could be used, but numbers should be limited to serve no more than 6 dwellings per courtyard. Under-croft could be an appropriate solution, but if this solution is proposed care is needed in order that the development creates an active frontage.

It will be necessary to provide safe and secure bicycle storage that meets the needs of future residents, which could be located in communal and/or private areas. Consideration should also be given to the provision of modern infrastructure such as charging points for electric vehicles.

Any works in the highway (including the erection of hoarding) or directly adjacent to the highway requires consultation with and approval from Traffic and Highway Services. Further information is detailed in the Traffic and Highway Services application form for undertaking works to a public highway.

### Archaeology

An initial assessment indicates that there is unlikely to be any significant archaeology surviving on the site. The majority of the site has previously had some form of development on it with almost the whole area under glass in 1938. Previous experience has shown that this tends to have a destructive effect on archaeological remains. It is therefore unlikely that any particular archaeological intervention would be required.

### Potential Of Demolition Of Existing Buildings

The majority of the site could be considered for redevelopment rather than the conversion or reuse of existing buildings. The replacement of the warehouses and former garage would provide an opportunity for positive enhancement of the area. The exception to this would be the former Late Shopper where its contribution to the street scene would need to be assessed on its merits having regard to Policy DBE9.

### Lifetime Homes Standards

It is important that housing developments cater for a range of housing needs for all members of the community, including elderly people and those with disabilities (Policies HO11, HO12 and HO13). Taking this into account, residential units should be designed for a range of users including over 55s (sheltered housing), social housing, key workers, rented and partial ownership. Proposals will be encouraged to meet Lifetime Homes Standards, particularly where sheltered housing is proposed. For further details of Lifetime Homes standards see Appendix B or visit [www.lifetimehomes.org.uk](http://www.lifetimehomes.org.uk)



# Appendix

# A

## Relevant Urban Area Plan Policies

Whilst individual policies of the Urban Area Plan (UAP) should not normally be taken out of context, the policies that are considered to be particularly relevant to the site and redevelopment for residential use are listed below. Potential developers/architects/agents are advised to refer to the specific policy wording in the UAP Written Statement in drawing up detailed development proposals.

UAP Policy	Policy Relevance
GEN1	The proposed development should benefit the community by restoring and enhancing a vacant site and providing new accommodation in a sustainable location. The sustainability of design and construction will be important considerations.
GEN2	The Development Brief provides guidance for the comprehensive development of the site.
GEN3	Improved landscape, ecological and wildlife features are required.
GEN4	The proposed development should enhance the urban environment and take account of local character.
GEN5	The proposed development should be of a good quality in terms of its design and the materials to be used and buildings should be of appropriate siting, layout and scale in relation to their surroundings.
GEN6	Account should be taken of locally distinctive features and characteristics of the local environment.
GEN7	Roads and infrastructure requirements should be considered carefully in the context of the adequacy of existing roads and utilities to cope with additional demand. The need for a Traffic Impact Assessment should be considered in respect of any future planning application for the site in accordance with guidance in Annex 6 of the UAP (if 25 or more dwellings are proposed). This would need to demonstrate any proposed access arrangements or suggested improvements to the road network would be satisfactory.
GEN8	The redevelopment should create an environment that provides for safe and convenient access for all users, including the needs of the mobility impaired. Convenient access could include additional dedicated pedestrian routes.
GEN9	Adequate levels of car parking and open amenity space should be integrated into the redevelopment in accordance with guidance outlined in Annexes 2 and 3 of the UAP. Where it is considered that a better redevelopment scheme can be achieved, this guidance will be interpreted flexibly.
GEN10	Any risk to public health and safety and the environment associated with the redevelopment of this site will need to be fully addressed as part of the consideration of any formal development proposals. An assessment of contamination will be required given the historic use of the site and proposals for amelioration.
GEN12	Care should be taken to ensure that the proposed development is not detrimental to the reasonable enjoyment of adjoining properties, for example by virtue of overlooking, overshadowing, emissions, noise or disturbance.
DBE1	The development should achieve a good standard of overall design and make a positive contribution to the urban environment.
DBE2	The development should make a positive contribution to the urban environment, in particular, strengthening the streetscape of the area. In view of its scale and location, it is necessary to prepare a Development Brief to guide potential developers of the site.
DBE4	The redevelopment proposals will be expected to incorporate a detailed landscaping scheme that reduces any impacts of the development, paying particular attention to the site boundaries with existing residential development together with a management programme to ensure future maintenance.
DBE9	Existing buildings and features that do not contribute to the character and appearance of the site will be considered for demolition. Replacement buildings will be required within a reasonable timescale to avoid the creation of a vacant gap site.
HO2	The policy provides a gateway for housing outside Settlement Areas on previously developed land provided the site is suitable in terms of existing characteristics of the site and its relationship with the surrounding area and is acceptable in terms of design, density and amenity.

H04	The Late Shopper building could offer the potential for conversion / subdivision for residential purposes, subject to achieving satisfactory standards for residential amenity, if it is considered worthy of retention.
H07	The residential buildings on site could offer the potential for conversion to low cost flats, houses in multiple occupation or staff hostels, subject to the provision of satisfactory living standards.
H09	Development proposals would be expected to increase the number and quality of housing units on site. A reduction in the number of units would not be acceptable.
H010	In encouraging higher density of housing development, the appropriate density for this site should take into account the characteristics of the site and surrounding area, the needs of occupiers and the level of amenity for existing and prospective residents.
H011	Any residential scheme should cater for a range of housing needs with the majority of a size, form and type suitable for smaller households (no more than two bedrooms - i.e. 4 habitable rooms or less).
H012	The needs of the mobility impaired should be addressed in the detailed design, particularly level access.
H013	The size and location of the site renders it suitable for the development of new sheltered housing.
EMP9	Suitable existing industrial sites should be retained for this use. However, the standard of accommodation, poor access and close proximity to residential development means that the garage premises are no longer suitable in land use terms to continued industrial use and such use has been dormant for an extended period.
WWM1	The redevelopment of the site should demonstrate adequate measures will be taken to prevent the risk of pollution to the water supply.
WWM4	Proposals must not cause any flooding off site.
WWM5	The redevelopment should include an acceptable surface water management plan, protecting the quality of the water supply and incorporating where appropriate sustainable urban drainage systems
CO1	The development would be considered an infill development on previously developed land within an existing group of buildings.
CO2	The existing buildings on the site could be reused. The site is brownfield in terms of policies HO2, and therefore suitable for redevelopment for housing, and redevelopment for a range of other potential uses.
CO3	Any new development should respect the distinctive landscape features of the area.
Annex 1	Sets out the requirements for the Development Brief.
Annex 2	Sets out the Parking Standards required, depending on the proposal.
Annex 3	Provides more detail on the issues to consider in terms of residential amenity.
Annex 6	Sets out the requirements for a Traffic Impact Assessment, should one be required, depending on the proposal.



# Lifetime Homes Diagram

## Appendix

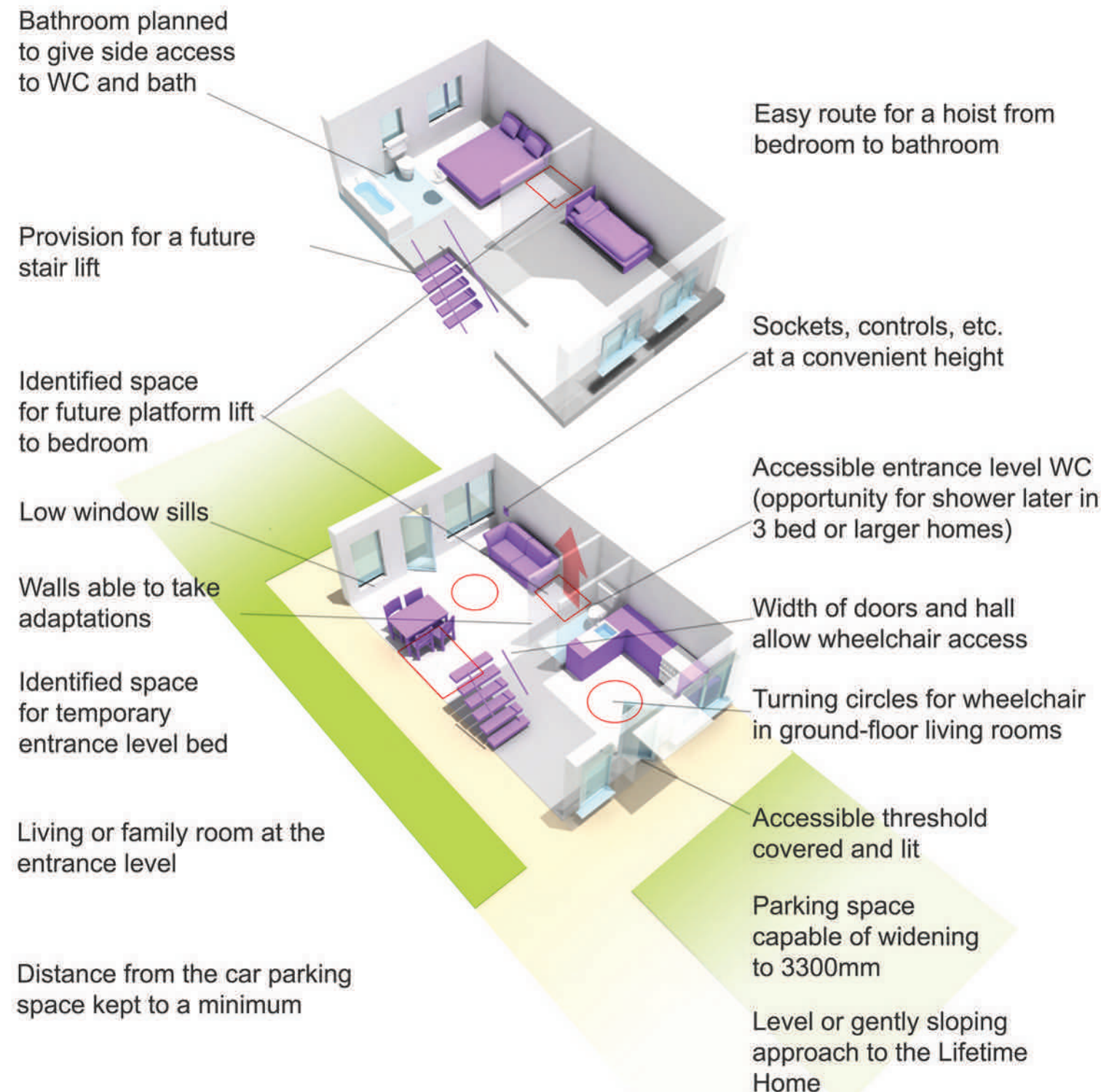
B

### Lifetime Homes Standards

The concept of Lifetime Homes was developed in the early 1990s by a group of housing experts, including Habinteg Housing Association and the Joseph Rowntree Foundation. The group was formed because of concerns about how inaccessible and inconvenient many homes were for large sections of the population. The principles of Lifetime Homes have supported the growing demand for choice, flexibility and independence among disabled people of all ages, as well as promoting high quality and thoughtful housing design for the general population.

Lifetime Homes are ordinary homes incorporating 16 Design Criteria that can be universally applied to new homes at minimal cost. Each design feature adds to the comfort and convenience of the home and supports the changing needs of individuals and families at different stages of life.

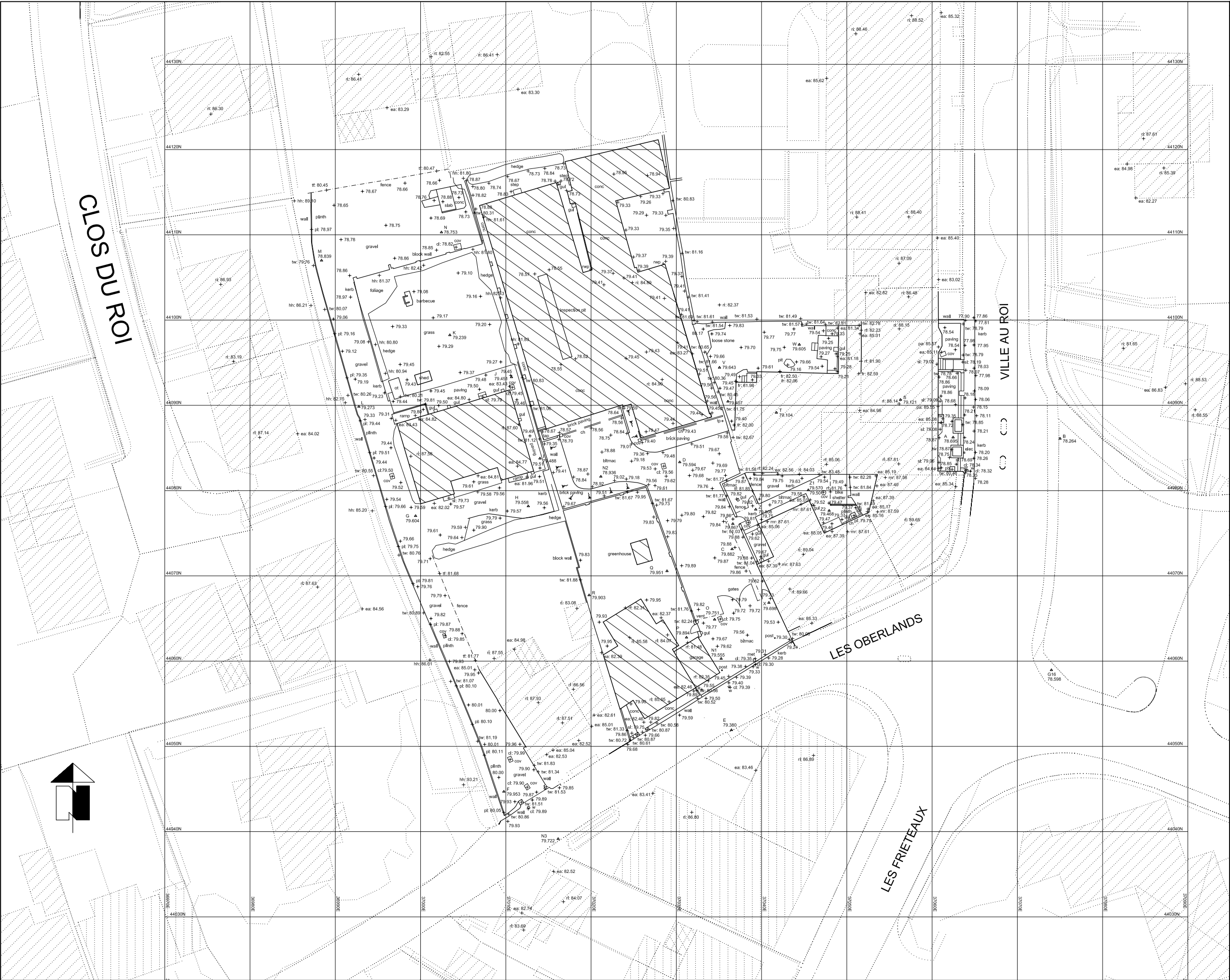
Lifetime Homes are all about flexibility and adaptability; they are not 'special', but are thoughtfully designed to create and encourage better living environments for everyone. From raising small children to coping with illness or dealing with reduced mobility in later life, Lifetime Homes make the ups and downs of daily living easier to manage.



Produced by PRP  
Diagram indicative only

© Crown copyright 2008





NOTES

1. Co-ordinates are in metres in Guernsey Grid.

2. Levels are in metres above Guernsey Datum.

3. Map data is shown dotted and has not been surveyed.

4. Map data is copyright © States of Guernsey 2015. User number 30.

LEGEND

ch = drainage channel  
cl = cover level  
conc = concrete  
cov = service cover  
ea = eaves level  
elec = electricity service  
fr = flat roof level  
gul = drainage gully  
hh = top of hedge/foliage level  
mr = mansard roof level  
ot = oil tank  
pa = parapet level  
pb = post box  
pl = plinth level  
rl = roof level  
rl = ridge level  
rwp = rain water pipe  
sl = step level  
tf = top of fence level  
tp = telecom pole  
tw = top of wall level

REVISIONS

A - 11/01/16, Detail in back garden of property to north of former shop and to rear of Amballa added, MHAM.

Marquis Surveys Limited  
Chartered Land Surveyors  
Chartered Engineering Surveyors

PO Box 391  
St Peter Port  
Guernsey  
GY1 3FJ

Tel: 01481 716669  
Mobile: 07781 163352  
Email: msl@cwgsy.net

Regulated by RICS

PROJECT

The former Priaux's Garage site

The Oberlands

St Peter Port

Guernsey

CLIENT

Guernsey Housing Association LBG

TITLE

Topographical survey merged with map

SCALE	DATE	DRAWN
1:200 @ A1	16/12/15	MHAM
JOB NUMBER		DRAWING NUMBER
150908B		002A



