



Definitions

GR2016-03



ASSOCIATED DOCUMENTS:

GR2016-01	Highway Opening & Reinstatement
GR2016-02	Utility Apparatus
GR2016-04	Approved Highway Contractors
GR2016-05	Guidance Notes
GR2016-06	Standard Highway Details
GR2016-07	Applications For Highway Works
GR2016-08	Working Near Trees & Removing Tree Roots



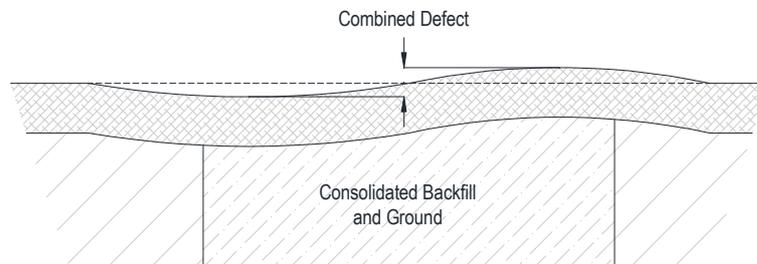
The definitions below are intended to help the interpretation of document GR2016-01 and have been expressed accordingly. Underlined text indicates that they are defined within this document.

Acceptance Criteria	The criteria used by <u>Traffic and Highway Services</u> to assess if any reinstatement is acceptable. If acceptable, <u>Traffic and Highway Services</u> will agree to take responsibility for its future maintenance, once the <u>Guarantee Period</u> has elapsed.
Administration Costs	The administration costs to <u>Traffic and Highway Services</u> incurred as a direct result of the Applicant's actions or omissions, inclusive of staff time spent arranging remedial works, allowing for staff on-cost. All such expenses will be charged back to the Applicant.
All-In-One Reinstatement	A single-stage reinstatement, where an <u>Interim Reinstatement</u> is not undertaken and no <u>Settlement Period</u> is allowed for. The final surface is provided immediately following the backfilling of the excavation and will be subject to a <u>Guarantee Period</u> of 5 years.
Applicant	Any person or body which requires written permission from <u>Traffic and Highway Services</u> in order to either undertake, or instruct on their behalf, works to be carried out within the <u>Public Highway</u> .
Approved Contractor	A body or person certified by <u>Traffic and Highway Services</u> as being approved to undertake specific works within the Public Highway.
Asphalt Surface	Asphalt bound material that comprises the <u>Surface Layers</u> of a highway, commonly called Tarmac or Bitmac. The technical name for material used in Guernsey is <u>Asphalt Concrete</u> .
Asphalt Reinstatement	Any reinstatement that uses Asphalt Concrete.
Backfill	Refers to the material used to refill an excavation beneath the <u>Sub Base</u> , <u>Road Base</u> and <u>Surface Layers</u> . <u>Backfill</u> material may be the excavated material, unless contaminated or deemed unsuitable. Any material must be adequately compacted during backfilling to stop subsequent <u>consolidation</u> ; otherwise the reinstated surface will likely fail.
Binder Course	The structural layer of the <u>Asphalt Surface</u> , between the <u>Surface Course</u> and the <u>Foundation Layers</u> . It both regulates and provides a stable base for the Surface Course. This was previously known as the base course. AC20 asphalt will be used as binder course.
Deferred Set Material	See Cold Laid Asphalt
BS EN 13108	British Standard regarding Bitumen laying practices.
BS 7533	British Standard regarding Modular laying practices.
Carriageway	The area of the Highway that vehicular traffic is permitted to drive on: however, other forms of traffic are permitted to use this area.
Cold-Laid Asphalt	A bonded aggregate mix designed to be laid cold, it remains workable until laid and then hardens (either due to compaction, air curing or some other mechanism). Generally these products do

not achieve the same degree of bonding, compaction or cohesion as Hot-Laid Asphalt. Only HAPAS approved materials may be used.

Combined Defect

A Combined Defect is an area within a reinstatement where any combination of Edge Depression, Surface Depression and/or Crowning, overlap and is measured from the top of one defect to the bottom of the other. This differs from UK method of measurement.



Compaction

The mechanical removal of voids through the use of whacker plates, rollers, rams or such like. This forces the material together, improving interlock and increasing strength. Compaction should be induced incrementally in layers as backfilling occurs. The effectiveness of such methods is dramatically reduced the thicker the material being compacted gets. See also Consolidation below.

Consolidation

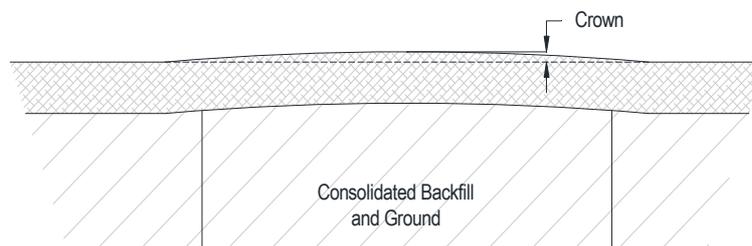
When trafficked or left for long periods, fill material, ground or other material will often settle, especially if Compaction has been poorly undertaken. Any failures due to Consolidation are entirely the responsibility of the Applicant.

Contractor

The contractor is the body who undertakes the actual work within the Highway. They are responsible to the Applicant, and the Applicant is responsible to Traffic and Highway Services.

Crowning

A raised reinstatement that is proud of the surrounding surface. Crowns can cause surface water drainage problems, ride quality issues, and tripping hazards, and as such may fail the Acceptance Criteria.



Cycle Track/Lane

An area of Highway provided for cyclists as either part of the Carriageway or within the Footway, and any works to Cycle Lanes should be reinstated to the standard of the adjoining Carriageway or Footway.

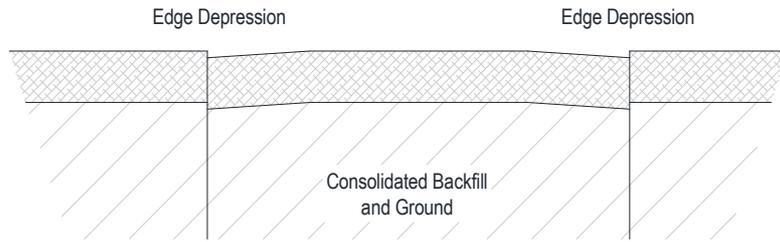
Delamination

Delamination is the separating of either the Surface Layers from the Foundation Layers or separation between the Surface and

Binder Courses within the Surface Layers. At all times the Surface Layers should be cut back beyond such failures during repairs or reinstatement.

Edge Depression

A lip or vertical edge against a trench edge or adjacent cover.



Embargo Period

The Embargo Period is a temporary restriction on non-essential works (works which are neither Emergency, nor Urgent Works), following Traffic and Highway Services undertaking Major Works. Currently this period is 3 years. The purpose of the embargo is to extend the life of the highway, and encourage more long term planning from Applicants.

Emergency Works

Work required to prevent an *immediate* danger to persons or property, or the failure to maintain an existing statutory obligation. The Applicant must notify Traffic and Highway Services, The Office of the Committee for the Environment and Infrastructure, and The Island Police of the need to undertake Emergency Works in accordance with IRIS procedures.

Excavation

Any work to open, break up, disturb, tunnel or bore into any public highway, its surface, its foundation or earth below, the removal of fixed features, or undermining of the surface.

Final Reinstatement

The final reinstatement is the permanent surface laid after the Settlement Period (and replaces the temporary surface of the Interim Reinstatement). After the Guarantee Period of 3 years, subject to compliance with the Acceptance Criteria, the Applicant will be not be obliged to undertake remedial works.

Fine Material

The small particles which may be present in soil or graded aggregates. During the pumping of surface or other water these must be trapped to prevent entry to the drainage network (and subsequent choking of the drainage network).

Fixed Feature

Any rigid component within the surface of the Highway. This includes ironmongery, kerbstones, channel blocks, previous reinstatements and road furniture.

Flexible Reinstatement

See *Asphalt Reinstatement*

Footway

A Footway is the area of Highway over which pedestrians have priority. Commonly called footpath, pavements or walkway.

Foundation Layers	The layers, below the Surface Layers, which include the Road Base, Sub Base and Backfill, these should distribute the <u>traffic loading</u> evenly to provide a stable foundation to the Surface Layers.
Full Width Reinstatement	Within an Embargoed Roads and in certain other situations, a Full Width Reinstatement may be required. This will be required if the excavation crosses the centre of the carriage way or the carriageway width is less than 4.0m. Most <u>footways</u> will require a full width reinstatement unless agreed with <u>Traffic and Highway Services</u> .
Green Lane	A Green Lane is generally an unpaved, or partially paved <u>Footway</u> or Carriageway and it is not considered a Public Thoroughfare.
Guarantee Period	The period where the Applicant maintains the reinstatement free of defects. It is to ensure a long term and durable reinstatement is undertaken. Where workmanship, materials or practices are below the required standards it is likely that signs failures will generally become apparent within this period. The Guarantee Period is 3 years following finalisation of an Interim Reinstatement and 5 years following an <u>All-In-One Reinstatement</u> .
Half Width Reinstatement	Within Embargoed Roads and in certain other situations, a Half Width Reinstatement may be required. This will be permitted if there is an existing centre joint and the excavation does not cross that joint. This will be instead of a <u>Full Width Reinstatement</u> .
HAPAS	The Highways Authorities Product Approval Scheme (HAPAS) which is a nationally recognised approval scheme for innovative highways products in the UK. Products used on Guernsey's Public Highways must have a current HAPAS certificate.
Highway Inspector	<u>Traffic and Highway Services'</u> representative charged with monitoring that reinstatements and other highway works are carried out in a manner compliant with GR2016-01 "Opening & Reinstatement of Traffic and Highway Services (2016)".
Highway Structure	This includes the Surface Layers, <u>Foundation Layers</u> and the features that support the highway or its edge.
Highway Service Life	The expected life of the road surface before major resurfacing works is required. Excavations or bad reinstatements reduce this service life.
Hot-Laid Asphalt	A bitumen / aggregate mix laid at temperatures around 125 °C ±5 °C, in accordance with <u>BS EN 13108</u> . If mixed at too high a temperature or laid at too low temperature then optimal cohesion, compaction and bonding will not occur and its service life will be significantly reduced. See also <u>Cold-Laid Asphalt</u> .
Interim Reinstatement	An interim or temporary reinstatement, which on completion of the Settlement Period, is then made good with a <u>Final Reinstatement</u> . An interim reinstatement is not subject to so stringent Acceptance Criteria as the <u>Final Reinstatement</u> or All-In-One Reinstatement.

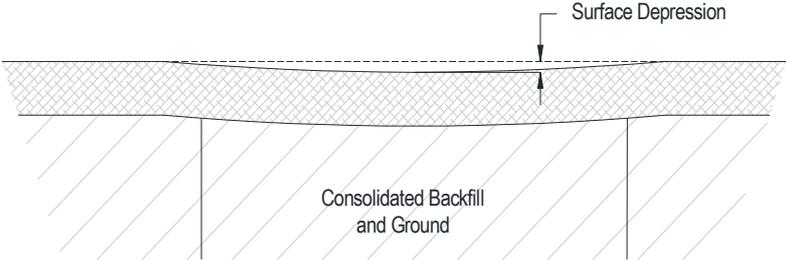
IRIS	The Island Roadworks Information System (IRIS) as administered by the <u>The Office of the Committee for the Environment and Infrastructure</u> in order to coordinate road works and approve associated traffic management measures for the works. This does not give the <u>Right to Excavate</u> in the Public Highway.
IRIS Application	An application requesting the The Office of the Committee for the Environment and Infrastructure to introduce traffic management or control, in order to work within the public highway. This does not give the applicant the right to excavate in the public highway.
Lead in Period	The 6 month period leading up to any <u>Major Works</u> scheme scheduled by <u>Traffic and Highway Services</u> . During this Lead in Period, Public Highways are subject to similar restrictions as roads under embargo. This is to reduce the risk of settlement occurring following any resurfacing works.
Lead Applicant	An Applicant who has submitted an <u>IRIS application</u> and has been designated by the The Office of the Committee for the Environment and Infrastructure to be responsible for the works where there may be more than one Applicant working within a section of road.
Loose Ground	Loose Ground is any material forming the sides of an excavation incapable of supporting itself (which therefore needs some form of support). It may also have poor load bearing characteristics often exacerbated by ground water.
Major Works	<u>Traffic and Highway Services</u> defines its Major Works as any large scale programmed Resurfacing to the Public Highway.
Minor Excavation	Minor excavations include reinstatements around covers of less than 500mm wide and a reinstatement area not exceeding 0.5m ² .
Modular Surface	Any surface consisting of blocks, bricks, slabs, cobbles, setts or other paviments.
Module	A Module refers to any single block / unit within a <u>Modular Surface</u> .
Narrow Roads	Any two-way Section of <u>Carriageway</u> less than 5.2m wide, or any one way section less than 2.8m, excluding any official parking area.
Narrow Trench	A trench of 300mm surface width or less, with a surface area greater than 1m ² . Extra care will be required to achieve proper compaction.
NJUG	National Joint Utilities Group. For information regarding NJUG see <u>www.NJUG.org.uk</u>
Public Highway	Any <u>carriageway</u> , <u>verge</u> or <u>footway</u> of any road, street, lane, alley or passage repairable in whole or in part by the States, including those that <u>Traffic and Highway Services</u> is responsible for maintaining as Public Thoroughfares.

Reinstatement	Generic term referring to works to make good a Highway following damage or excavation. See also Interim Reinstatement and <u>Final Reinstatement</u> .
Remedial Action	Action required to be undertaken due to a failure to comply with the Acceptance Criteria.
Resurfacing	Resurfacing refers to the renewing of the Surface Course of a defined area of the Highway. This may involve the planing out of the old surface or over laying the existing surface; it may also include renewing of the Binder Course and <u>Foundation Layers</u> .
Right to Excavate	Either a statutory right or written permission from <u>Traffic and Highway Services</u> to undertake an excavation in the Public Highway.
Road Base	A base layer beneath the Surface Layers, to provide increased strength to the roads structure. This can be made of concrete, asphalt, compacted stone, or other material, but where encountered must be replaced like for like. See also <u>Foundation Layers</u> .
Road Humps	Speed bumps/sleeping policemen, or similar. Where present must be reinstated to the agreed profile.
Rumble Strips	Raised strips generally warning of potential hazards. Where present these must be reinstated as agreed.
Service Owner	Any <u>Applicant</u> , <u>Utility</u> or other organisation, including <u>Traffic and Highway Services</u> , which owns <u>Apparatus</u> within the <u>Public Highway</u> .
Settlement	Settlement refers to consolidation of material within the <u>Foundation Layers</u> , after an excavation has been backfilled and surface reinstated. This is generally due to poor compaction procedures or voids being left.
Settlement Period	Following the backfilling of an excavation and undertaking an Interim Reinstatement, the period to allow settlement to occur due to traffic loading, prior to the <u>Final Reinstatement</u> being undertaken. Typically this is 6 months but may be extended by Traffic & Highway Services.
Shared Works	Shared Works refer to either a joint road closure between Applicants, or a shared excavation.
Small excavations	Excavations or openings with a surface area of 1m ² or less.
SRV	Skid Resistance Value as determined by the Transport Research Laboratory
Statutory Undertaker	See Utility
Sub Base	Granular material typical laid on the subgrade (<i>natural ground material</i>) to form the first layer of the foundation also referred to as GSB (granular sub-base).

Suitable Backfill Suitable backfill comprises material capable of evenly supporting the loading above on a permanent basis. It should be capable of adequate compaction and not susceptible to be degraded by ground water or the movement of material.

Surface Course Refers to the uppermost surface of an asphalt road, upon which vehicles and pedestrians travel on. The Surface Course will be either 6mm or 10mm down asphalt.

Surface Depression A dip within the reinstatement or immediately next to it compared to the surrounding surface. Surface Depressions can cause surface water drainage problems, ride quality issues, and tripping hazards, and as such may fail the Acceptance Criteria. Depressions are most likely due to insufficiently compacted of the excavation or voids left at the excavation edge.



Surface Layers Surface Layers refers to the Surface Course and Binder Course in an asphalt surface, and the Modules and bedding material in a modular surface.

Surround to Apparatus A material used to surround and protect cables and ducts. Typically stone dust, chippings or similar.

Traffic & Highway Services A section of **The Office for the Environment & Infrastructure** that is tasked to undertake highway maintenance, the inspection of highway works, and execute statutory requirements contained in the Public Thoroughfares (Guernsey) Law 1958.

The Highways Unit of Traffic and Highway Services is responsible for maintenance and inspection of the public highways.

The Traffic Unit of Traffic and Highway Services is responsible for managing traffic flow around the Island and the administration of the IRIS system.

Traffic Loading The weight or load applied to the Highway Structure by moving vehicles, which over time may lead to additional compaction of a backfilled excavation or material and subsequently lead to settlement.

Traffic Sensor/Loop These are cables buried in the surface to detect vehicles. These are normally located near traffic signal junctions. Typically these will be damaged during excavation and will require replacing at the Applicant's expense.

Traffic Sign	Any road or other sign erected by or with the approval of The Office <i>of the</i> Committee <i>for the</i> Environment and Infrastructure.
TRL	Transport Research Laboratory, where products are independently tested to find specific properties (such as Skid Resistance Value). For more information see www.TRL.co.uk
Utility	Any organisation that, by law, has the right to place apparatus within the highway subject to conditions. Utility Companies include Guernsey Electricity, Guernsey Water, Guernsey Gas, Sure and Jersey Telecom.
Utility Apparatus	Refers to any of the Applicant's equipment, apparatus, features, ironmongery or assets, either buried beneath, present within, or adjacent to the surface of the public highway.
Verge	Verges are areas outside the trafficked Highway, which may be slightly raised (but not a bank or cutting) and are generally grassed.
Wear Course	See Surface Course.