



draft Route Isabelle Development Framework

Supplementary Planning Guidance March 2018

Contents

1. Introduction..... 3

2. Site Overview 3

3. Planning Policy Context 3

4. Surrounding Area 4

5. Site Description 6

6. Site Analysis..... 9

7. Development Guidelines 11

8. Waste Management Plan and Construction Environmental Management Plan 14

Appendix 1: IDP Policy Context.....15

1. Introduction

1.1. This Development Framework provides planning guidance for the potential residential development of land on the north side of Route Isabelle, St Peter Port. It provides broad, comprehensive and practical guidance on how the policies in the Island Development Plan (IDP) will be applied to the site, and sets out an appraisal of the site and wider area. Once approved, this Development Framework will become Supplementary Planning Guidance and any planning application for residential development on the site should be in accordance with it.

2. Site Overview

2.1. The site is located in St Peter Port, within the Main Centre Outer Area. The site area is approximately 0.36 hectares (2.2 vergées). The site is accessed directly from Route Isabelle, and is on the edge of the Main Centre Outer Area where the dense built form of St Peter Port Conservation Area meets the open countryside. To the north and west the site adjoins a number of residential properties, with further residential and commercial buildings visible as a backdrop to the north and north east on the valley crest. To the east the site adjoins agricultural fields and the villas of Les Granges De Beauvoir (Ivy Gates), whilst on the opposite side of Route Isabelle to the south is the Foulon Cemetery. Image 1 shows the site outlined in red on an extract from the IDP Proposals Map.

3. Planning Policy Context

3.1. In the IDP, this site is located within the St Peter Port Main Centre Outer Area. The site is not designated as a Housing Allocation Site and therefore IDP policies state that the site could in principle be developed for a range of alternative uses. This Development Framework has been prepared to guide residential development, but does not in itself preclude the submission of planning applications for other uses.

3.2. The 2014 Strategic Housing Land Availability Assessment deemed the site to be deliverable with no known factors or constraints to development. Residential development of the Site is supported by the IDP.

3.3. This Development Framework therefore provides planning guidance for residential use of the site, and, in accordance with policy MC2: Housing in Main Centres and Main Centre Outer Areas, is required to guide proposals of 10 or more dwellings or for sites exceeding 0.25 hectares (1.5 vergées) in area. In itself this Development Framework does not require that planning applications for residential development on this site are for 10 units or more, but given the size of the site, a framework is required.

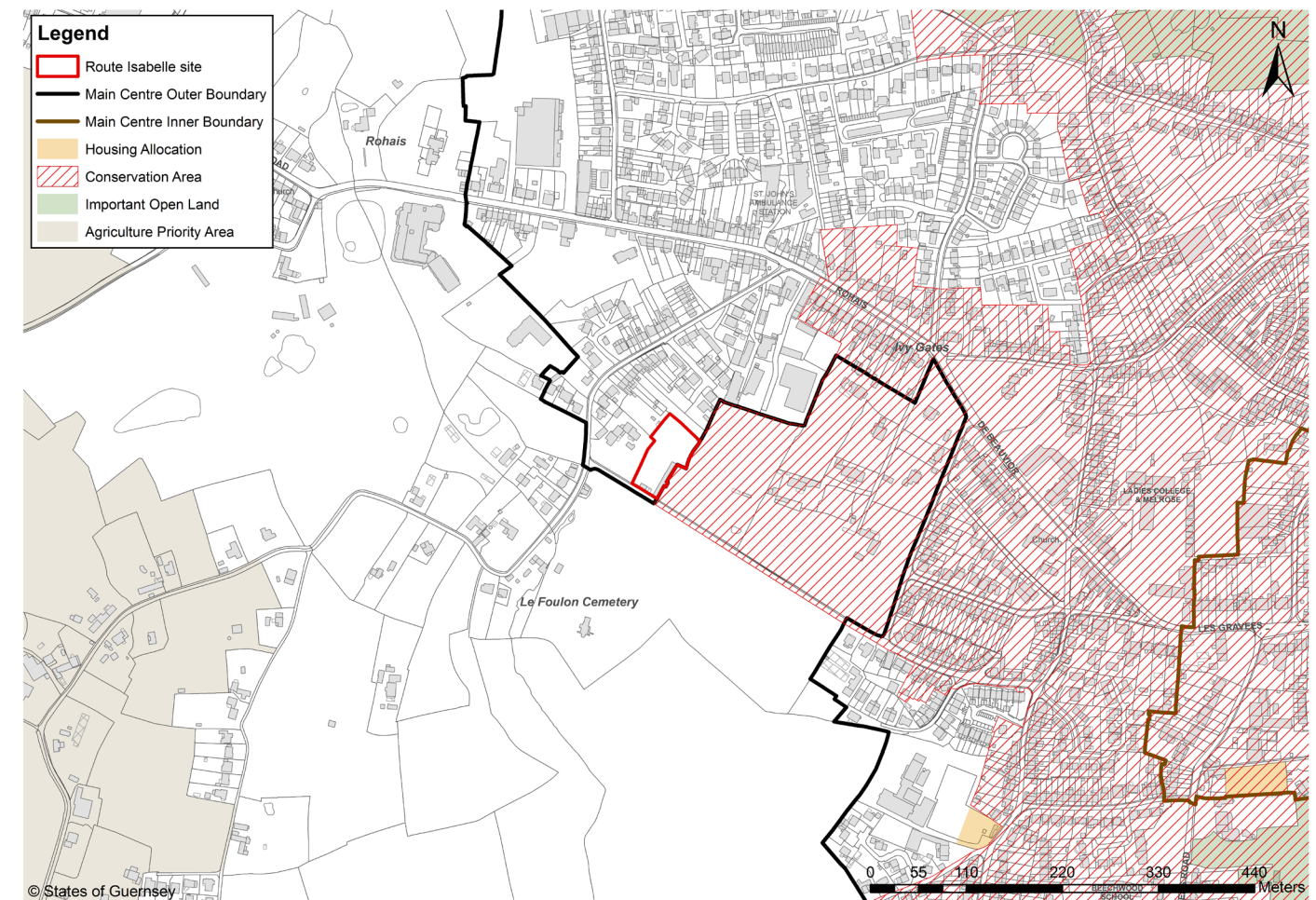


Image 1: Extract from the IDP proposals map with the Route Isabelle site outlined in red.

3.4. Whilst individual policies of the IDP should not normally be read in isolation or taken out of context, the policies that are considered to be particularly relevant to the site are listed in Appendix 1 with a summary.

3.5. There is also relevant Supplementary Planning Guidance (SPG) for Parking Standards and Traffic Impact Assessment.

4. Surrounding Area

4.1. The site and surroundings are located on the outskirts of St Peter Port, and are described as being rural in the Guernsey Character Study, June 2013. The site is located within the upland landscape of the Foulon Valley, with valley and escarpment characteristics as detailed in Annex V: Landscape Character of the IDP. The site is visible in public views across fields to the east and from within the Foulon Cemetery to the South. There are no protected buildings, protected monuments or protected trees on site or in the surrounding area and the site lies outside the St. Peter Port Conservation Area, but its eastern boundary is on the edge of the Conservation Area. No sites of archaeological importance have been identified in the local area, and there are no Sites of Special Significance or Areas of Biodiversity Importance, though this does not mean that there is no potential for archaeological finds or biodiversity on site. There is no identified risk of flooding to the site.

4.2. The character and density of built development in the surrounding area is mixed, including nineteenth century terraces and detached farmhouses, mid-twentieth century bungalows and houses, and larger commercial buildings. Building heights range from single to 2 ½ storeys. New residential development was approved on the adjacent site to the west in 2016, and building work has recently commenced.



Image 2: Photo location map



1. Small scale bungalow clos, Le Foulon Clos, Low density development away from urban area



2. Granite 19th century terrace houses on Foulon Road high density steep pitched traditional form, closer to urban centre



3. Rendered 1950/60 bungalows along Foulon Road low density, set of three dwellings.



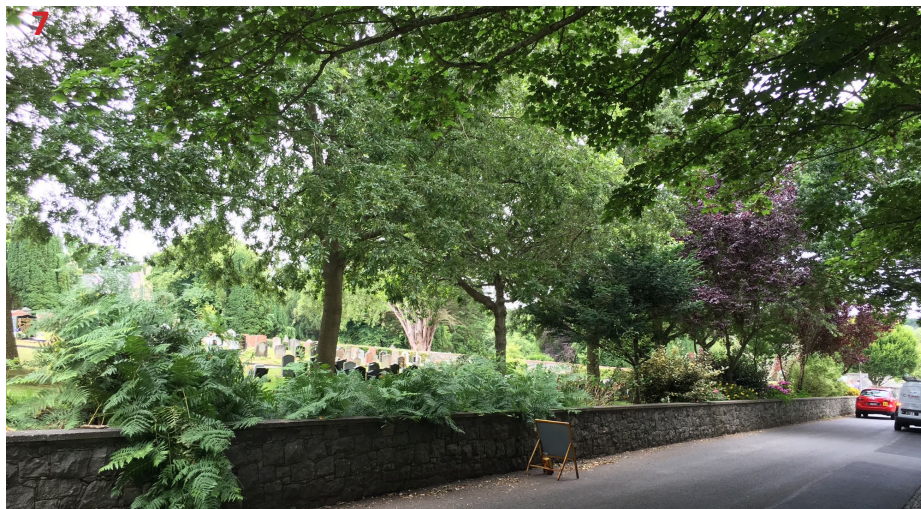
4. Neighbouring farmhouse Le Foulon Farm. 19th century traditional Guernsey granite and natural slate development on large site. Low density rural character



5. Development visible from east edge of site at Route Isabelle. Traditional materials and characteristics such as twin gable and granite boundary walls.



6. Neighbouring arable field from route Isabelle (North). Bordered by trees and hedges. Currently agricultural land. Glimpses of small scale, low density traditional housing can be seen across the field



7. Le Foulon Cemetery from Route Isabelle, meets the road with traditional granite wall and is screened by trees and hedging.



8. Low density rural development featuring 19th century cottages. Traditional material palette of white rendered walls with pantiled roofs.



9. More agricultural land to north of Route Isabelle. Views of typical development can be seen across.



10. Route Isabelle towards east. Agricultural fields give way to more urban development of St Peter Port Main Centre Outer Area.



11. Open land for growth of Le Foulon Cemetery. Strong screening with row of trees. Traditional granite wall retains increasing level change towards east.



12. Land slopes away from north edge of Route Isabelle

4.3. The site is located within walking and cycling distance of the wide range of shopping, social and community services and facilities on offer within St Peter Port.

4.4. The site access is from Route Isabelle which is classified as a Neighbourhood Road. Access to the nearby Traffic Priority Route on the Rohais is via Le Foulon and York Avenue.

4.5. Pedestrian pavements offer safe walking to destinations on the Rohais or east into St Peter Port town centre. The Rohais is serviced by bus routes 41, 42, 51 and 52, and bus stops are a short walk from the site.

4.6. Analysis of residential development on sites of a similar scale in the locality shows an average density of around 33 dwellings per hectare.

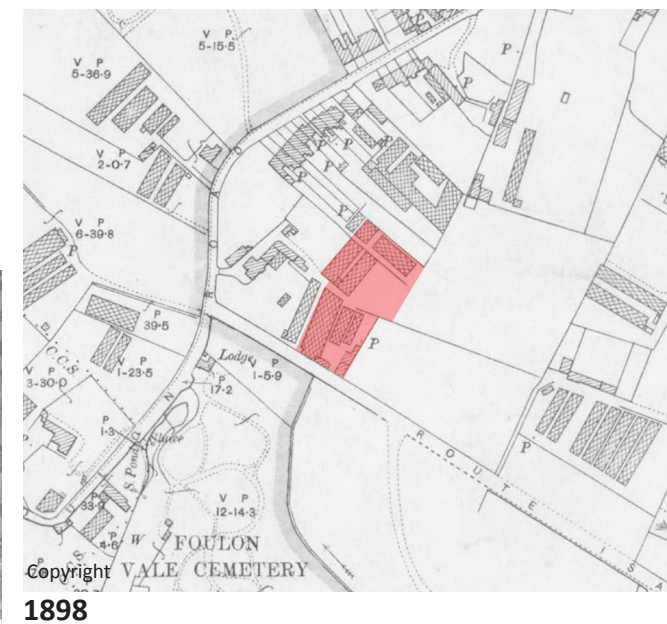


Image 3: Local cluster development comparison

5. Site Description

Current Use

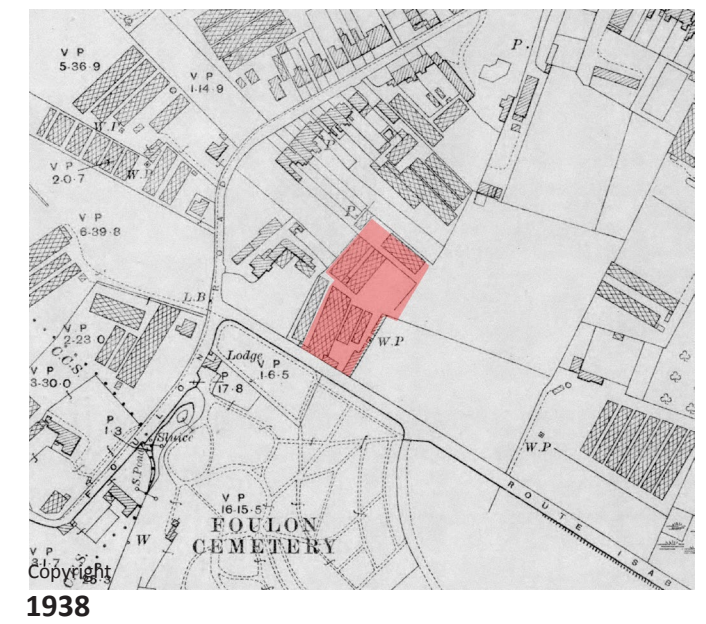
5.1. The recognised and lawful use of the front (southern) part of the site is as a haulage/packing yard, with the remainder having reverted to agricultural use on demolition of the former glasshouses. The current low level use of the site for the storage of builders equipment, materials and vehicles is considered to fall under that historic established use.



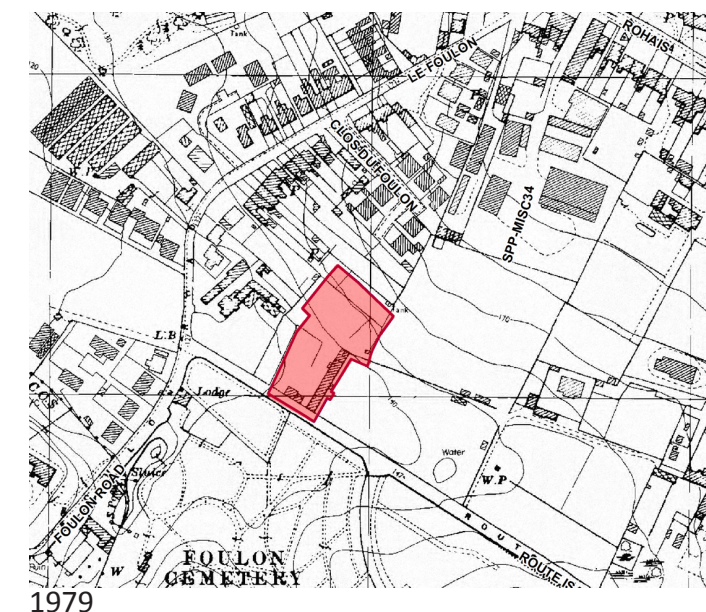
Glasshouses and ancillary buildings occupy the majority of the site, probably for horticultural use in connection with Le Foulon Farm.



Only one of the glasshouses has survived. A water tank has been erected in the north-east corner of the site.



A number of minor changes have taken place including the construction of additional buildings on the east boundary and the removal of a small glasshouse.



All glasshouses have been removed. Majority of site (approx. 75%) is vegetated with grass and trees, with only a small area used for commercial purpose.

Existing Buildings

5.2. A range of single storey lean-to workshop/storage buildings stand adjacent to the vehicular access in the south east corner of the site.

Existing Landscaping and Biodiversity

5.3. Substantial mature trees and established hedging exist along the north and part-west site boundaries, whilst a number of smaller trees exist along part of the east boundary. The majority of the site is currently given over to rough grassland, with an area of hardstanding in the southern part of the site adjacent to the existing buildings and site access.

5.4. The map data from the 2010 Habitat Survey of Guernsey, Herm and Associated Islands details the site as containing both bare ground and brownfield areas. Brownfield land is common on old vinery sites and offers a good habitat to ruderal plants until out-competed by species established in later successional stages.

Image 4: Site section AA

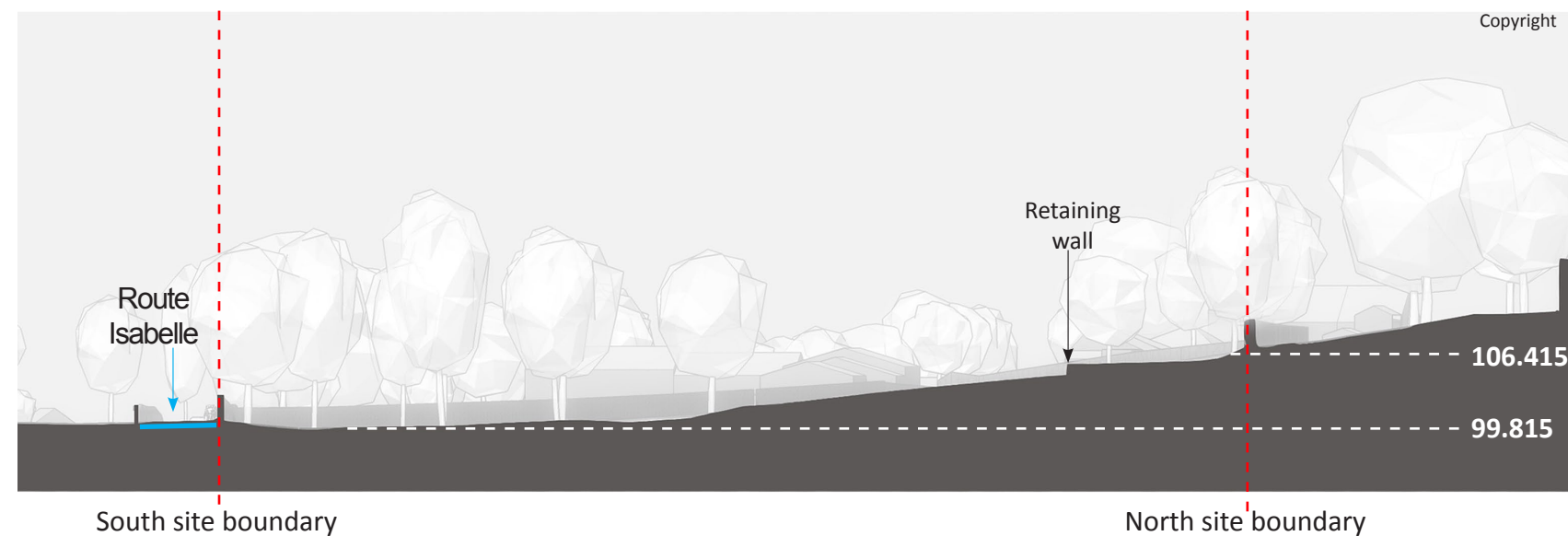
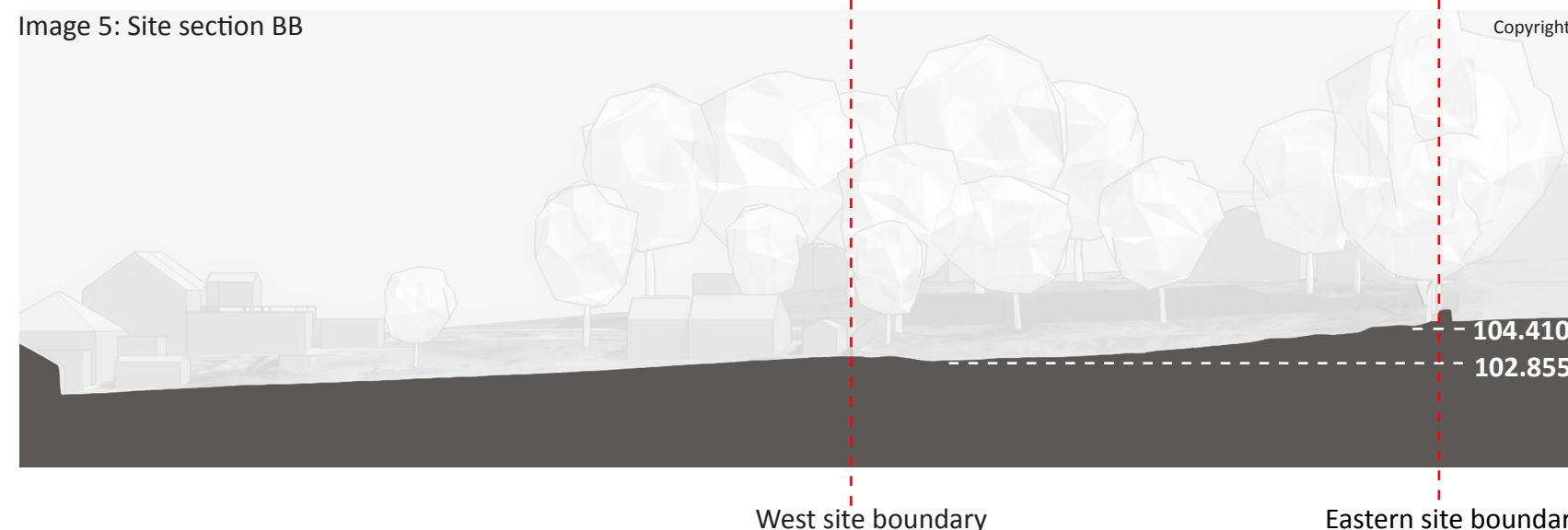


Image 5: Site section BB



Boundaries

5.5. The roadside boundary on Route Isabelle is defined by a c.1.8m tall granite wall, which also extends along part of the east site boundary. The west, north and part-east boundaries are defined by mature trees, hedging and close boarded timber fencing.

Site Access

5.6. Vehicular access is from Route Isabelle via a 4.5m wide opening in the roadside boundary wall.

5.7. There is a right of way across the site serving the neighbouring property currently known as "Argyll".

Land Levels

5.8. At the site access there is a short ramp down from road level which quickly flattens out. The land then rises gently at first and increases significantly in gradient until reaching a small granite retaining wall which marks a 1.0m step up in level at the uppermost part of the site. Overall the land rises 5.5m from Route Isabelle to the northern boundary.

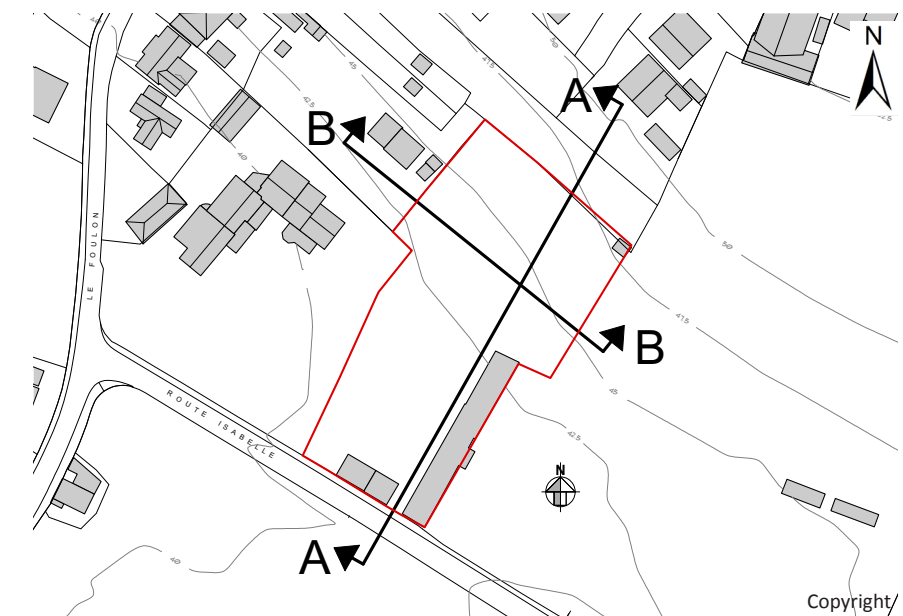


Image 6: Site section locations

Public Utilities

Foul Water Drainage

5.9. There is an existing 15 inch diameter terracotta public sewer located in Route Isabelle at approximately 2.0m below road level to invert and falling east to west. The sewer has provision for future connection via branches. Note that there is an existing main drain connection to the site.

Surface Water Drainage

5.10. Surface water drainage discharges in to the stream at the bottom of Route Isabelle near the entrance to Paulo's Plant Centre.

5.11. Guernsey Water advise that the surface water drain pipe is shallow and therefore it is likely that some works will be required to upgrade the system.

5.12. Discharges in to this system should also be attenuated to the greenfield runoff rate of 3 litres per second per hectare.

Mains Water Supply

5.13. The site is currently served by a well and therefore a new service provision will be required for this development.

5.14. Guernsey Water have advised that for residential development, a mains extension into the site is required. Each dwelling will connect to this extension through an individual 25mm MDPE service.

Mains Electricity Supply

5.15. Service provision will be subject to details of load required for each dwelling. This can be varied according to supply constraints.

5.16. Guernsey Electricity have advised that the mains cable will need to be extended from Le Foulon Road to serve the proposed development.

Telephone Connection

5.17. Overhead line network available via telegraph pole in south-west corner of site.

5.18. Provision to be made for telephone cables to be routed underground via ducting within the site.



Image 7: Map showing photo locations



1. View from north to south of site. Land slopes up from Route Isabelle towards north of site.

6. Site Analysis

6.1. Proposals for development need to take into account the constraints and opportunities that the site presents so that the design and layout respond to the specifics of the site. The main constraints and opportunities of this site are described below and these are acknowledged within the Development Guidelines in the next section;

- The site adjoins land with existing and planned buildings/gardens on its north and west boundaries;
- Development on this site could lead to overshadowing and overlooking to existing and planned neighbouring dwellings;
- Existing buildings on the site do not make a positive contribution to the character and appearance of the area;
- There is no uniform or dominant architectural style in the locality, with both traditional and more modern building types evident;
- The topography of the site is such that development is likely to become more prominent on the higher ground in the northern part of the site;
- The existing site access is flanked by tall granite walls and consequently affords relatively poor visibility to vehicles exiting the site;
- Development on this site could result in visual impact on longer distance views across adjacent agricultural land and from the Foulon Cemetery opposite the site which contains areas that are sensitive in terms of visual amenity and noise;
- Existing mature trees on the site make a positive contribution to landscape character, although overall biodiversity appears to be relatively low;



2. View of site entrance and pedestrian pavement from Route Isabelle



3. High granite walls and site entrance on Route Isabelle.



4. View from site across neighbouring field on Route Isabelle.



5. Area of site used as builders yard



Image 8: Site analysis diagram

7. Development Guidelines

7.1. The following guidelines must be read in association with the preceding analysis, and planning applications relating to the site should respond appropriately to the constraints and opportunities identified.

Density and Housing Numbers

7.2. Planning applications relating to the site must involve a comprehensive scheme for the whole of the site in order to make the most effective and efficient use of land, in accordance with policies GP8 and GP10. The density and form of development shall be designed to integrate well with the overall character of the surroundings. New dwellings will be expected to respect the form, bulk and massing in the vicinity and be of no less than 1 ½ storeys in height.

7.3. Securing an efficient and effective use of the developable part of the site will be highly dependent on the design response to other criteria and to material planning considerations. For example, satisfactory and appropriate architectural design and scale, consideration of potential impacts on the amenities of neighbouring residents, and the provision of suitable amenity space for future residents. Based on the identified character of the site and surrounding area, it is likely that a low density residential development of between 20-30 dwellings per hectare (providing a yield of between 7 and 11 dwellings for the site) would be appropriate. However, it is important to note that this density and yield are indicative. The site may be able to yield more, or less dwellings, subject to meeting all the development and design guidelines as set out in this Framework.

Mix and Type of Housing

7.4. Policy MC2: Housing in Main Centres and Main Centre Outer Areas requires an appropriate mix and type of dwellings to be provided. The mix and type of dwellings should be reflective of the demographic profile of households requiring housing based on the most up to date evidence available, balanced with the need to respect the surrounding character of the area and the requirement to demonstrate the most effective and efficient use of land. The exact mix will be determined at the point of any planning application on the site, informed by the most up to date information such as Housing Needs Studies and other relevant information held by the States of Guernsey.

7.5. Developments which result in a net increase of 20 or more dwellings are required to provide a proportion of the developable land for affordable housing in accordance with Policy GP11.

Siting and Design

7.6. Planning applications relating to the site will be expected to promote a layout of development that relates well to the transitional character of the site and its surroundings, and to the layout and pattern of development in the vicinity, in accordance with Policy GP8. This may result in development that fronts onto

Route Isabelle, but could also involve development in depth. Buildings should not be sited unduly close to existing mature trees as this could damage their roots, reduce natural light to rooms and gardens, and lead to future pressure for their removal.

7.7. Levels across the site may need to be rationalised, but consideration should also be given to the potential to integrate the existing topography as part of any development to minimize the need for excavation and fill across the site. Proposals will also require designs to resolve accessibility across the site despite level changes.

7.8. The architectural appearance of any future development should draw from and respond to the character of the surrounding buildings, though design need not replicate the surroundings. Consideration should be given to the finishes and materials as well as typical forms of detailing used in the area, whilst not precluding the use of contemporary detailing and use of familiar materials to the streetscape. There may be opportunities to consider alternative materials inside the site to create a harmonious contrast.

7.9. Planning applications relating to the site will be expected to demonstrate how the design, method of construction and location of proposals to help to achieve more sustainable development has been considered at the earliest stages of the design and development process, in accordance with Policy GP9. Sustainable development also includes accessibility to and within buildings for people of all ages and abilities and offering flexible accommodation able to respond to people's needs over time.

7.10. Surface water drainage should be dealt with on site to reduce the flood risk to properties lower down Route Isabelle. It is recommended that any paving to the access road is permeable and the use of Sustainable Urban Drainage Systems are encouraged to ensure that surface water run-off is controlled effectively.

Potential Effect on Neighbours

7.11. The interrelationships between buildings should be considered from the outset and the design should respect the amenity of adjacent properties. Measures such as buffer zones could be established on the site to protect adjacent properties and planned developments that would otherwise have current or future amenities affected by the development of the site, particularly the property to the west (Le Foulon Farm). A mixture of landscape, planting and spatial buffers should also afford privacy to new dwellings on the site, in accordance with Policy GP8.

Access, Parking and Transport Infrastructure

7.12. Planning applications relating to the site should provide a single point of access onto Route Isabelle with visibility splays, layout and design which meets the minimum Traffic Engineering Guidelines for Guernsey and Part P of the Building Regulations. This could either be through utilising and adapting the existing access or by relocating the access (see image 9). Safe access must also be provided for pedestrians and cyclists to the site.

7.13. In accordance with policy IP7: Private and Communal Car Parking, and Parking Standards Supplementary Planning Guidance, appropriate levels of parking must be provided on site for cars, motorcycles and bicycles. Maximum parking standards are set out on pages 6 and 7 of the Supplementary Planning Guidance. Cycle parking should be considered at the outset of the design process and be covered, secure and easily accessible in order to comply with policy IP6: Transport Infrastructure and Support Facilities. Proposals must also demonstrate compatibility with policy IP9: Highway Safety, Accessibility and Capacity where the road network must be able to cope with the increased demand resulting from the new development. Lighting to the access road should be considered from the outset and carefully designed to meet requirements whilst not causing light pollution in this otherwise rural area.

7.14. Planning applications relating to the site must provide for continued access to the neighbouring property currently known as “Argyll”.

Landscape Design

7.15. In accordance with policies GP1 and GP8, planning applications relating to the site should incorporate a carefully considered landscaping scheme of both soft and hard landscaping in order to respect the character of the surrounding area, to enhance the amenity of the residents on the site and to enhance the potential biodiversity of the area. The landscaping needs to be fully integrated into the overall scheme design to ensure a suitable external environment is created that will improve the natural habitat of the area, whilst providing for the needs of the residents in the form of a practical design that will develop comfortably over time. Appropriate species for tree planting may include Ash, Silver Birch, Sweet Chestnut, Lime, English Oak, Scots Pine. Appropriate hedging species may include Blackthorn, Elder, Hawthorn, Holly.

7.16. Existing granite walls on the south and east site boundaries which make a positive contribution to the character of the locality should be retained where possible in providing improved vehicular access to the site.

8. Site Waste Management Plan and Construction Environmental Management Plan

8.1. Development Frameworks are required to include an outline Site Waste Management Plan specific to the site in question. A detailed Site Waste Management Plan is required at planning application stage.

8.2. Site Waste Management Plans apply to all aspects of a project, with the majority of opportunities for waste minimisation existing at the design phase. Information should be provided with a planning application on the amount and type of waste that will be produced during the course of a project and how waste will be reduced, reused, recycled, recovered or disposed of. This should be a living document, drafted from the conception of a project and being added to and evaluated until the completion of the development and submitted again to the Authority prior to occupation or use of any dwelling on the site.

8.3. All materials from the demolition of the buildings and removal of the hard surfaced areas should be carefully sorted, separated, and distributed accordingly through the appropriate routes for recycling, recovery or disposal, in order to minimise the waste produced. Any excavated top-soil should be stored on site for re use. Subsoil where possible should be reused on site.

8.4. The final design will incorporate dedicated waste and recycling storage and collection provision on the site suitable to the density of the development approved.

8.5. A Construction Environmental Management Plan (CEMP) is unlikely to be required but consideration of this will be given at the planning application stage..

Appendix 1: IDP Policy Context

IDP Policy	Policy Relevance
S1: Spatial Policy	The spatial policy in the IDP sets out that the Main Centres and Main Centre Outer Areas have the role as the focal point for development to maintain the vitality of these areas.
S2: Main Centres and Main Centre Outer Areas	The Main Centres provide the core focus for development within the Island and proposals for development in these areas will generally be supported. Proposals for development within the Main Centre Outer Areas will also generally be supported where this would not detract from the objective of ensuring the Main Centres remain the core focus for economic and social growth. In both cases proposals must meet the requirements of the relevant specific policies of the Island Development Plan.
MC2: Housing in Main Centres and Main Centre Outer Areas	This policy requires the Development Framework for the site which, once approved, will be taken into account when considering proposals for the site. The mix and type of dwellings provided on the site is expected to be reflective of the demographic profile of households requiring housing. Therefore information such as the latest Housing Needs Survey and any other information held by the States of Guernsey relevant to this issue will be considered. Given the overall capacity of this site, a variety of sizes and types should be included.
GP1: Landscape Character and Open Land	This policy sets out that development will be supported where it respects the relevant landscape character type within which it is set, where development does not result in the unacceptable loss of any specific distinctive features that contribute to the wider landscape character and local distinctiveness of the area, and takes advantage where practicable of opportunities to improve visual and physical access to open and undeveloped land. A landscaping scheme will be required for this scale of development.

GP8: Design	<p>Development, including the design of necessary infrastructure and facilities, is expected to achieve a high standard of design which respects, and where appropriate, enhances the character of the environment. Two or more storey buildings constitute a more efficient use of land than single storey buildings and therefore development proposals should consider a multi-storey design from the outset, unless there are overriding reasons why this design approach would be unacceptable. Proportionate residential amenity space must be provided appropriate to the housing type and location.</p> <p>Development must respect the character of the local built environment and provide soft and hard landscaping to reinforce local character and/or mitigate the impacts of development including contributing to more sustainable construction. The amenity of occupiers and neighbours is also important – see IDP Annex I for further information.</p> <p>Residential accommodation is required to be accessible for all and to be flexible and adaptable. Proposals will need to demonstrate that they have been designed in such a way that design features that support people being able to live in their own homes for as long as possible can be easily added in the future when required.</p> <p>Development should also provide adequate individual or communal areas for storage of refuse and recyclable materials</p>
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GP9: Sustainable Development	<p>The policy is wide-ranging and includes requirements for sustainable design and construction with reference to the design, layout and orientation of buildings, flood risk and surface water run-off, renewable energy and use of materials. Hard landscaping should include the use of permeable paving and other Sustainable Urban Drainage Systems (SUDS). The design of soft landscaping can also help address drainage and run-off issues in accordance with this policy.</p> <p>Development of 5 or more dwellings will require a Site Waste Management Plan. It should consider the re-use/disposal of arisings from demolition of any existing buildings on site.</p>
GP10: Comprehensive Development	Any proposal will be required to be a comprehensive development and make the most effective and efficient use of the land.
GP11: Affordable Housing	<p>Developments which result in a net increase of 20 or more dwellings are required to provide a proportion of the developable land for affordable housing.</p> <p>The most up-to-date Housing Needs Survey or Housing Market Survey, as well as the Housing Waiting Lists (available from the States of Guernsey Housing) will be used to determine the type and tenure of affordable units required.</p>
GP18: Public Realm and Public Art	<p>Any proposal should consider the relationship between the development and the public realm and should enhance where possible.</p> <p>The inclusion of public art as an integral part of a development or as a standalone feature will be encouraged.</p>
IP1: Renewable Energy Production	Proposals for renewable energy installations (and ancillary and associated development) will be supported where they can be satisfactorily incorporated into the built form of the proposed development.
IP6: Transport Infrastructure and Support Facilities	Development should encourage a range of transport options and should be well integrated with the transport network. Development is required to provide appropriate levels of bicycle and motorcycle parking in accordance with the Supplementary Planning Guidance: Parking Standards and Traffic Impact Assessment.

IP7: Private and Communal Car Parking	<p>The parking standards for the IDP are set out in the Supplementary Planning Guidance: Parking Standards and Traffic Impact Assessment. For residential development in the Main Centre Outer Areas the car parking standards are in section 6. The standards are maximums and the development would be expected to be within these standards.</p> <p>Provision will need to be made for secure covered bicycle parking for residents and visitors.</p>
IP9: Highway Safety, Accessibility and Capacity	The public road network's ability to cope with increased demand, physical alterations required to the highway, and the access requirements of all people will be considered.
IP11: Small Scale Infrastructure Provision	Proposals for small-scale infrastructure provision will be supported where this would contribute to the maintenance and support of efficient and sustainable infrastructure, the applicant being required to demonstrate that the sharing or co-location of facilities, buildings, apparatus and support structures is not practically possible.

Contact Us for further information and advice at: Planning Service, Sir Charles Frossard House, St Peter Port. GY1 1FH Telephone 01481 717200 Email planning@gov.gg

Have you visited our website? Go to www.gov.gg/planningandbuilding for additional guidance material and other planning information, including how to book a pre-application discussion.

This SPG is issued by the Development & Planning Authority to assist understanding of the provisions of the Island Development Plan (2016) and, where applicable, relevant legislation, by offering detailed guidance but is not intended to be exhaustive or a substitute for the full text of legislation or the policies within the Island Development Plan (2016). This SPG does not form part of the Island Development Plan (2016). It represents the Development & Planning Authority's interpretation of certain provisions of States of Guernsey policy or legislation. The guidance is indicative of the Development & Planning Authority's likely approach to development proposals in relation to the site in question and is not binding on the Development and Planning Authority. This SPG does not prejudice the Development & Planning Authority's discretion to decide any particular case differently according to its merits and it does not relieve the Development & Planning Authority of any obligation, restriction or liability imposed by or under the Land Planning and Development (Guernsey) Law, 2005. Copies of the text of the Island Development Plan (2016) are available from Sir Charles Frossard House and also available electronically online at www.gov.gg/planningpolicy. Copies of legislation are available from the Greffe. Electronic copies are also available at www.guernseylegalresources.gg. Substantive queries concerning the guidance or a specific site should be addressed to the Planning Service by email at planning@gov.gg. The Development & Planning Authority does not accept any liability for loss or expense arising out of the provision of, or reliance on, any advice given. You are recommended to seek advice from an independent professional advisor where appropriate.