

# Braye Road Industrial Estate Development Framework

Supplementary Planning Guidance June 2021



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## 1. Introduction

**1.1.** This Development Framework provides planning guidance for the development of land to the east of the existing Braye Road Industrial Estate, which forms part of the wider Saltpans Key Industrial Area (KIA) and Key Industrial Expansion Area (KIEA) allocation as set out in the Island Development Plan (IDP, 2016). This Development Framework provides comprehensive and practical guidance on how policies in the IDP will be applied to the site. This Development Framework is Supplementary Planning Guidance and any planning application for development on the site should be in accordance with it.

**1.2.** The overall intention of this guidance is to facilitate high quality, well landscaped industrial development which respects neighbouring residential amenity, enhances the biodiversity of the site and responds sensitively to adjoining Protected Buildings.

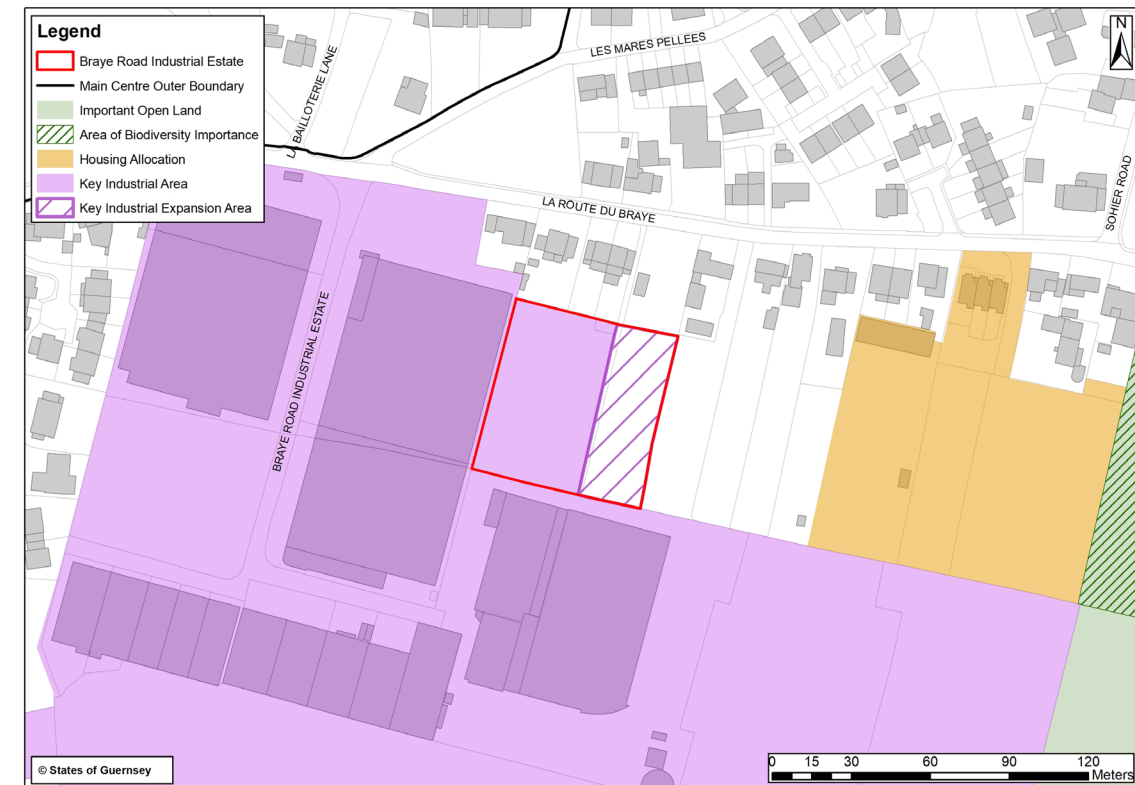
## 2. Overview and Policy Context

**2.1.** The site is located within an urban/built up area within the Vale Parish, falling within the Bridge Main Centre Outer Area. The site area is approximately 0.42ha (2.6 vergées). The western part of the site is allocated as part of the wider Saltpans KIA, and the eastern part is allocated as a KIEA, as shown on Image 1. Saltpans is one of four Key Industrial Areas identified in the Island Development Plan, and each has an identified Key Industrial Expansion Area. Because of the nature of the allocations, the site must be developed for industry, storage or distribution uses.

**2.2.** The purpose of the KIAs is to enable consolidation of industry, storage and distribution uses, with the KIEAs allowing for the expansion of these areas. Whilst the KIA part of the site can come forward for appropriate development at any time, development on the eastern part of the site KIEA will only be supported where no suitable alternative sites are available within the KIAs or Main Centre. The KIEA element of the site is included within this Development Framework to ensure that the whole site is planned in a comprehensive way so that future development of the KIEA is not precluded and the whole development is undertaken in a way that makes effective and efficient use of the entire site.

**2.3.** There are no existing buildings on the site, which was previously in horticultural use. Existing industrial development lies to the west and south with residential development to the north, including residential development under construction immediately adjoining the KIEA (see planning application FULL/2019/1127). To the east is an area of undeveloped land managed as amenity grassland (lawn) with residential gardens adjoining this to the east. A douit forms the southern boundary of the site. Image 2 shows the Development Framework site outlined in red on an aerial photograph from 2019.

**2.4.** To the east of the KIA allocation is an Area of Biodiversity Importance and Important Open Land, connected to the Development Framework site via the douit running along the southern boundary (see Image 1).



**Image 1:** Extract from the IDP Proposals Map, 2016, showing the site outlined in red.



**Image 2:** Aerial image (2019) showing the site outlined in red.



## 3. Site and Surrounding Area

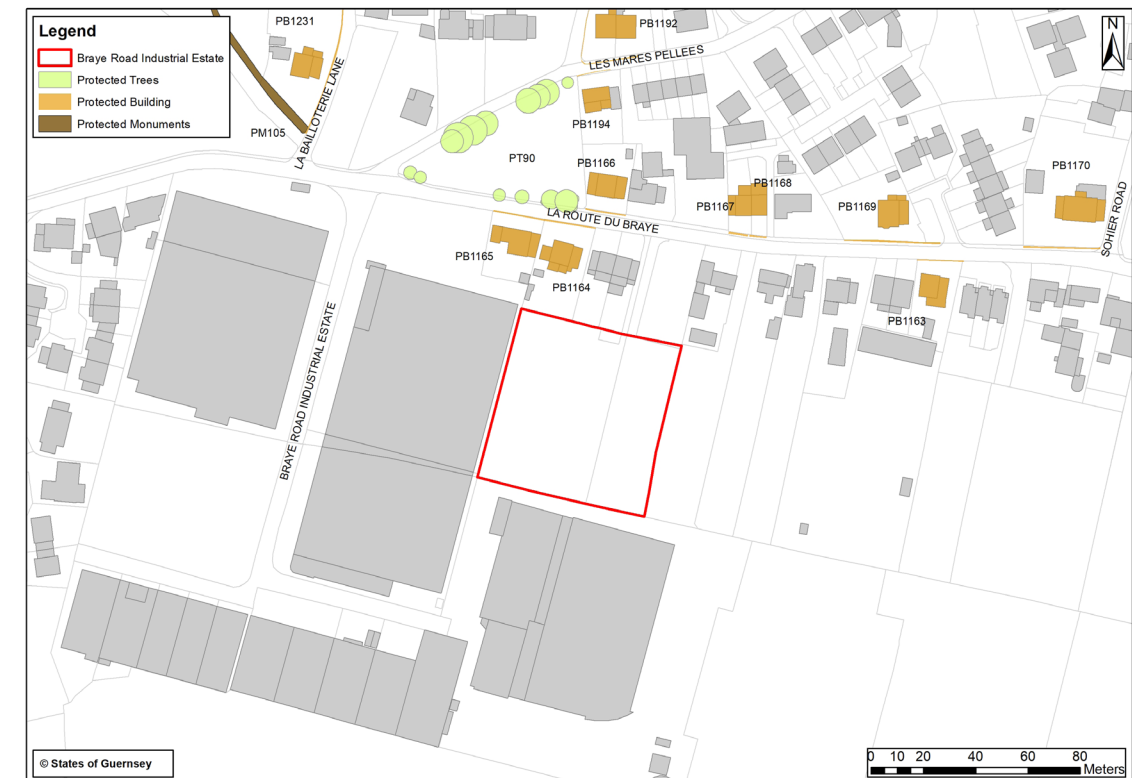
**3.1.** The site falls within a built-up area within the Lowlands Landscape Character Area within the Braye du Valle Wetlands. This area is of marine origin, covered by high tides until the whole area was drained in 1808, following construction of embankments at each end. This reclamation enabled the layout of straight roads and a gridiron pattern of field divisions.

**3.2.** There are a number of Protected Buildings in the area including one Protected Building adjacent to the site to the north (Brayehurst, La Route du Braye, PB1164). A Protected Monument lies to the north west on La Chasse des Quais (Old Sea Wall, PM105). A group Tree Protection Order (TP90) covers trees at La Pointe, again to the north west. There is no known archaeological interest on the site. The Protected Buildings, Protected Monument and Protected Trees in the area are shown on Image 3, and photographs 3 to 6 show some of those closest to the site.

**3.3.** The site was formerly in horticultural use with the glass being cleared some time between 1980 and 1996. There are no existing buildings on the site, although two electricity transformers which support the network and current industrial units lie within the site adjacent to the northern boundary. There are no significant changes in level across the site although there is a slight slope down from north to south and west to east.

**3.4.** In the 1999 and 2010 Habitat Surveys the western part of the site was recorded as Semi-Improved Grassland which is very species diverse and is of conservation value but according to the 2018 Habitat Survey the site is now managed as Improved Grassland which has a very low species diversity. The eastern part of the site was recorded as Semi-Improved grassland in 1999, and in 2010 it was recorded as dense scrub/continuous bracken but is also now managed as Improved Grassland.

**3.5.** The site is currently accessed from La Route du Braye which is designated as a Traffic Priority Route. La Route du Braye has relatively high traffic flows and a high proportion of HGV and commercial traffic accessing industrial estates, as well as commuter traffic. There are bus stops along La Route du Braye and a pedestrian footpath runs along its north side, however there is no pedestrian crossing to the Industrial Estate and no dedicated pedestrian infrastructure within the Industrial Estate. There are no bicycle paths in the area.



**Image 3:** Protected Buildings, Protected Monuments and Protected Trees in the area surrounding the site, shown outlined in red



**Photo 1:** View into the site from La Route du Braye with residential development in the foreground (not within the DF site)



**Photo 2:** View across the site from the south west corner



**Photo 3:** Brayehurst, La Route du Braye PB1165



**Photo 4:** Brayehurst, La Route du Braye PB1164



**Photo 5:** La Pointe, La Route du Braye TP90



**Photo 6:** Old Sea Wall, PM105



**3.6.** The Guernsey Press Building lies to the south of the site and is accessed via the Braye Road Industrial Estate. The road geometry around the building is 'tight' and vehicles are required to follow a one-way circuit around the building, passing alongside the southern boundary of the Development Framework site in order to egress the industrial estate. There are poor visibility splays at various points in this one-way circuit due to the proximity of surrounding buildings.

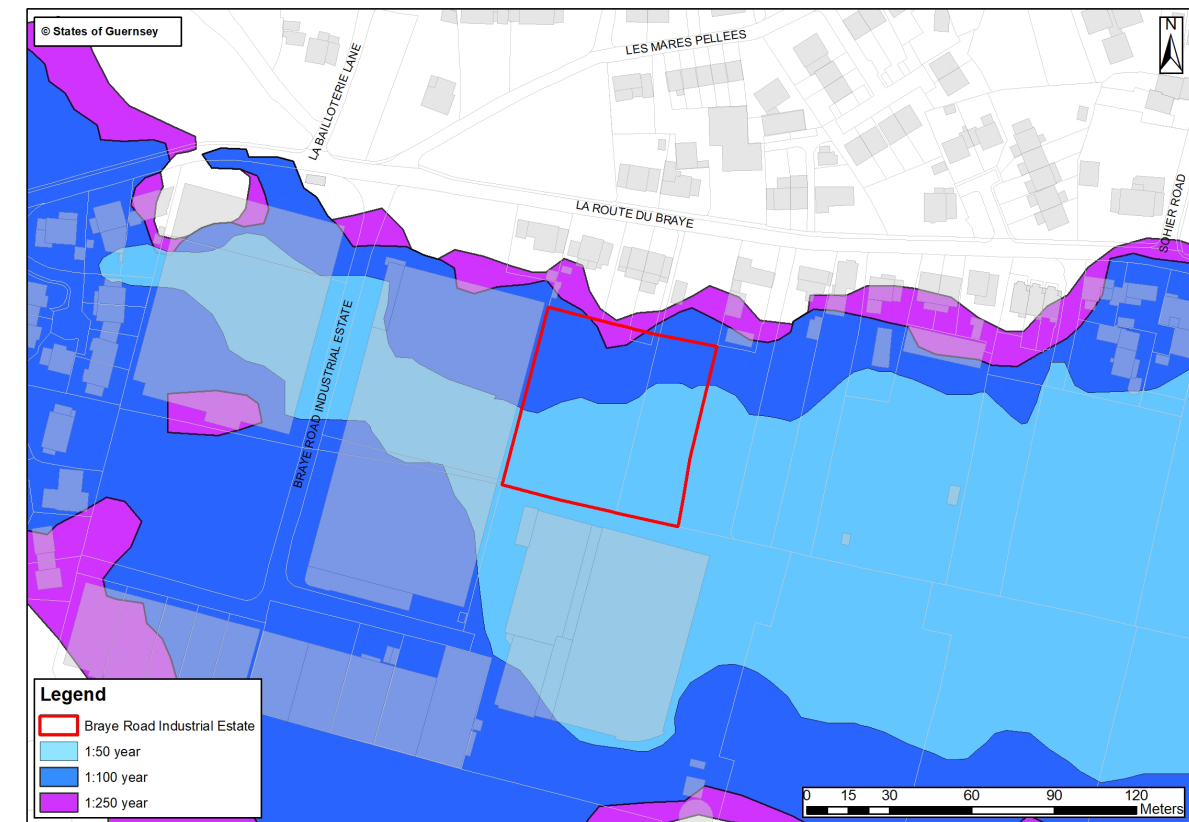
**3.7.** A douit forms the southern boundary of the site and connects the site to the Area of Biodiversity Importance and Important Open Land to the east beyond the KIA allocation. Within the site, part of this douit is open (towards the east), whilst a section at the western end is covered with concrete slabs.

**3.8.** The northern boundary of the site is formed of concrete fence panels, with the western boundary formed by a stone wall. Until recently mature vegetation grew alongside the western boundary wall but this has been removed. To the east, the boundary is marked by trees and shrubs.

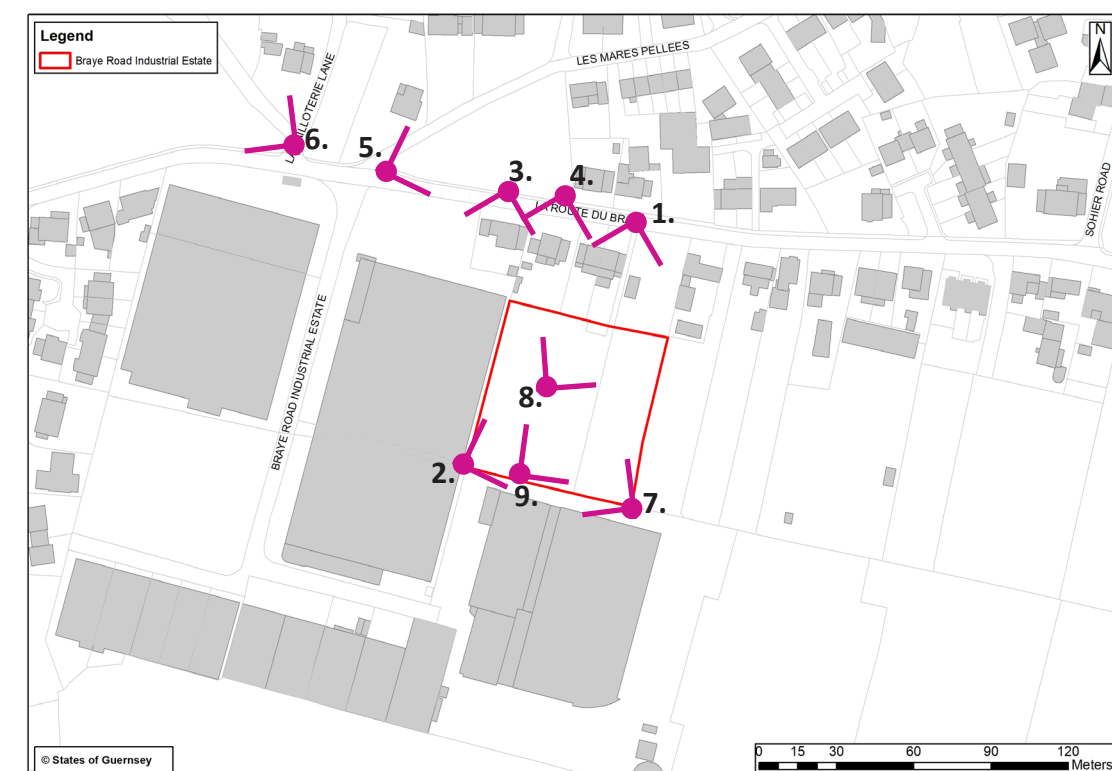
**3.9.** The water infrastructure to the immediate south of the site is private pipework that feeds the Guernsey Press site. Guernsey Water infrastructure terminates within the section of the estate road of Braye Road Industrial Estate that leads to the Guernsey Press site.

**3.10.** With regards to potable water, the metered zone within which the site falls is currently at capacity. Additionally, there is insufficient capacity to sufficiently feed any on-demand fire-fighting system. Capacity of foul water systems will depend on the specifics of any development.

**3.11.** Approximately half of the site falls within the 1:50 year flood risk, with the remainder of the site falling within a 1:100 year or 1:250 year risk. This risk originates from over-topping of the coastal defences at both The Bridge to the east and Le Grand Havre at the western end of the Braye du Valle. In addition to coastal flood risk, the potential for surface water flooding is also a concern. The douit is important in managing water flow in the area. Image 4 shows the extent of coastal flood risk in the area.



**Image 4:** Coastal flood risk for the site and surrounding area



**Image 5:** Location of photos 1-9



**Photo 7:** Douit forming the southern boundary of the site



**Photo 8:** Substation adjacent to the northern boundary of the site



**Photo 9:** Surface water on site

## 4. Site Analysis

**4.1.** Proposals for development must take into account the constraints and opportunities that the site presents. The main constraints and opportunities associated with this site are set out below:

### Comprehensive Development

**4.2.** Development of the site provides an opportunity to create an attractive, accessible development which is amenable to workers, surrounding residents and people visiting the site and which can meet the identified requirements of businesses.

**4.3.** Policies in the IDP require that the site should be developed in phases - the western KIA element followed by the eastern KIA in the future. This provides an opportunity to develop the site in two phases.

### Setting

**4.4.** In protecting residential amenity there will be constraints on the type, scale, layout and form of development permitted so that the amenity of neighbouring properties is not unreasonably affected by issues such as overlooking, overshadowing, noise nuisance, traffic nuisance or industrial smells.

**4.5.** The site makes a low to negligible contribution to the setting of the Protected Buildings PB1164 (Brayehurst) and PB1165 (Braye Lodge). This impact decreases further from the southern boundary of the Protected Buildings but may influence layout and design of development.

**4.6.** The site makes a negligible contribution to the setting of PM105 (Protected Monument - Old Sea Wall) via the association with the drainage of the Braye du Valle. Development of the site will not harm the setting of that Protected Monument. The site makes no contribution to PT90 (Protected Trees - La Pointe) and is not constrained by this feature.

### Access

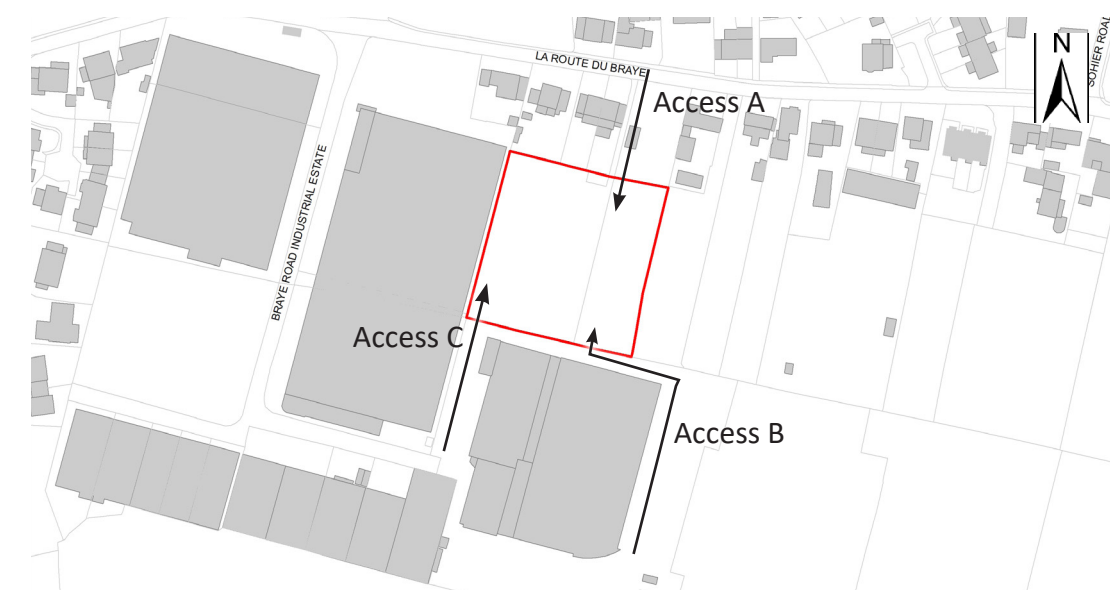
**4.7.** Vehicular access to the site is constrained by the configuration of the existing buildings on the Braye Road Industrial Estate to the west and south which effectively isolates the site from the broader Saltpans Key Industrial Area. There are three potential opportunities to access the site, however all are constrained.

**4.8.** Should the main access to the site be from the north via the residential access opposite Braye Road Garage, road safety and traffic management issues, as well as the technical specification of that road, will influence the size and type of development that is considered acceptable on the site (site access A, Image 6). Additionally, a vehicular access from the north could enable the creation of a 'cut-through' to the rest of the industrial estate which could lead to road safety and/or traffic management issues.

**4.9.** If the main access to the site is created from the south via the existing Guernsey Press site utilising the existing industrial estate access, this would require access from the single width road running to the north of the Guernsey Press site (site access B, Image 6). An access point to the site could be created in the southeast corner, linking to the Guernsey Press perimeter road, with an egress point in the southwest corner, re-joining the one-way Guernsey Press perimeter road, or a single point of access and egress from the Guernsey Press perimeter road could be created along the southern boundary of the site.

**4.10.** If the Guernsey Press perimeter road is utilised for access, the geometry in relation to the width of the perimeter road and the necessary turning arc of vehicles turning in and out of the site would need careful consideration. In all cases the use of bell-mouth accesses would be required and dependent on the internal road and building layout within the framework site, it is likely that a significant area within the site would be required to enable vehicles to manoeuvre in and out of the site safely.

**4.11.** A further alternative would be to utilise the Guernsey Press access but rather than following the existing one-way circuit, vehicles could potentially access the Framework site by going through the access gate to the Guernsey Press area and turning immediately left towards the southwest corner of the site (site access C, Image 6). This would negate the requirement for vehicles to circumnavigate the Guernsey Press building to access the Framework site, however it would require very careful planning to manage vehicles and avoid conflict between vehicles driving around the one-way Guernsey Press perimeter road and vehicles accessing and egressing the Framework site.



**Image 6:** Site access options A-C

**4.12.** Lack of pedestrian infrastructure within the Braye Road Industrial Estate creates a pedestrian road safety risk given the volume, type and frequency of movements through the Braye Road Industrial Estate. Improvements could be made to pedestrian infrastructure to ensure safe crossing from the pavement on the north side of Braye Road.



**4.13.** The flat roads in the area enable good opportunities for cycling to and from the site. There is an opportunity to include bicycle storage and facilities to encourage travel by modes other than private car.

## Design

**4.14.** There is an opportunity to create a high quality development with a sense of place through the inclusion of appropriate public art.

**4.15.** A proliferation of signs or inappropriate security measures such as chain link or steel palisade fencing would degrade the image and appearance of the area, however there are opportunities to ensure security and identity of the site by addressing such issues early in the design process to contribute positively to a sense of place and overall identity of the scheme.

**4.16.** Similarly, inappropriate lighting can create light pollution and nuisance from easily triggered Passive Infrared Systems. This potential nuisance could constrain the type and range of lighting systems considered suitable for the site.

## Landscaping and Biodiversity

**4.17.** The recently removed mature vegetation along the western boundary contributed to the biodiversity of this site. There are opportunities to mitigate this loss through robust native landscaping.

**4.18.** The douit to the south of the site potentially supports wildlife. The covered section of the douit could be opened up and restored to open stream to enhance biodiversity and contribute to public realm, as well as contributing to flood risk management. This could create a wildlife corridor to connect with other habitats and ultimately the Area of Biodiversity to the east.

**4.19.** There are opportunities to enhance and create new wildlife areas, for example by restoring pockets of grassland to species-rich grassland or wild-flower meadows. Other opportunities to increase biodiversity on the site include sensitive native landscaping and include bat and bird boxes.

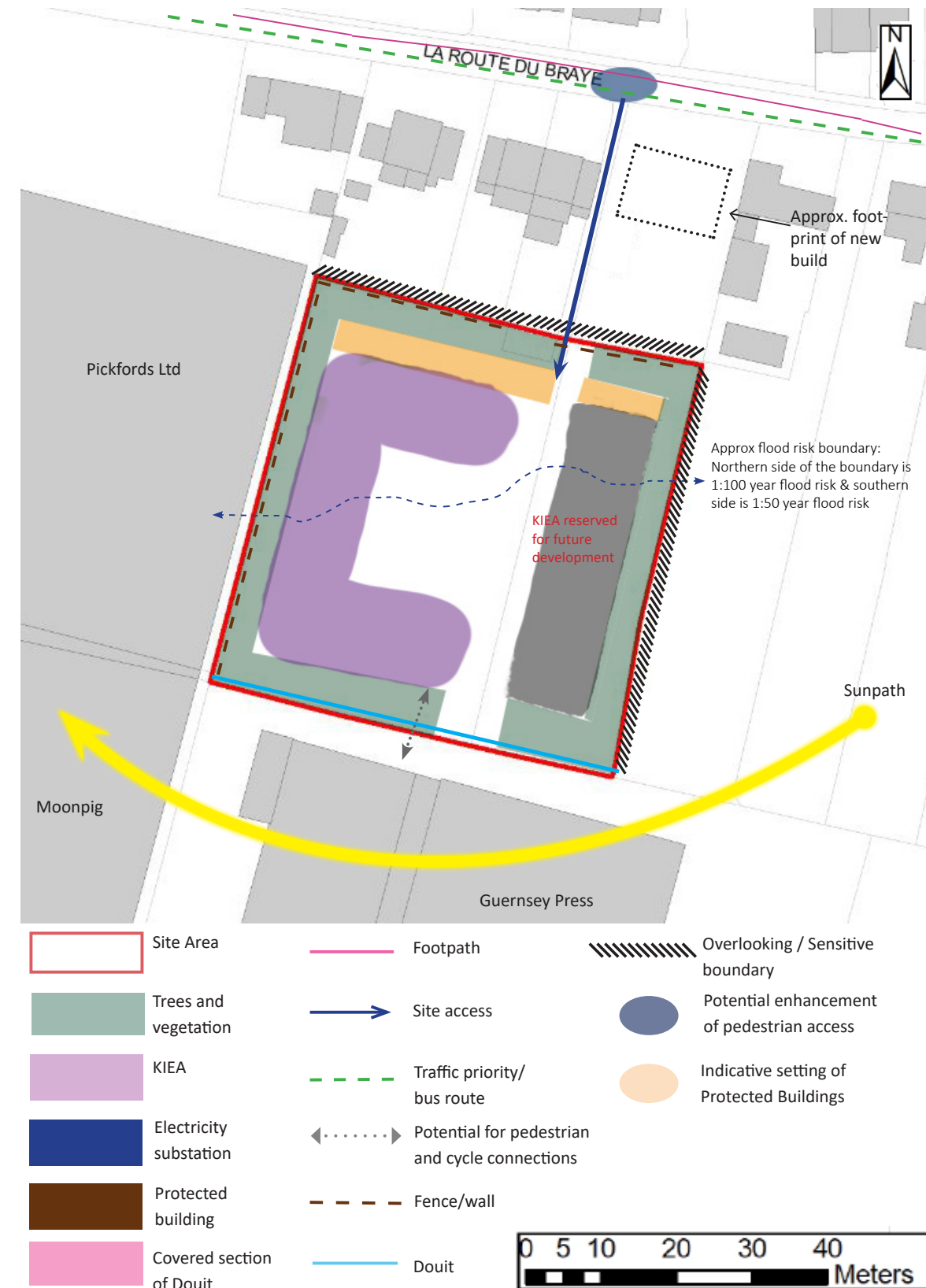
## Flood Risk, Environment and Utilities

**4.20.** Coastal and surface water flood risk associated with this site will influence the design of development, as well as water management, in order to ensure resilience to climate change and flooding.

**4.21.** Retention of the douit to the south of the site may influence layout of development. The douit also represents a sensitive receptor which may influence construction and drainage.

**4.22.** The capacity of the potable water infrastructure and on-demand fire-fighting system will need to be addressed and the impact on foul water systems modelled to ensure there is sufficient capacity. In addition, telecommunication providers have confirmed that there is capability to provide sufficient fibre capacity to serve the site.

**4.23.** Given the previous history of horticultural use the site could be contaminated.



**Image 7:** Site analysis diagram

## 5. Development Guidelines

**5.1.** The following guidelines must be read in association with the preceding analysis and context. Achieving the high quality of development required by the IDP requires the following development guidelines to be used to inform a creative and iterative design process that brings together all of the guidelines to create a positive development that respects its context.

**5.2.** All proposals for the site will need to support the States's of Guernsey recognition of the climate emergency by demonstrating how they have considered and adopted the State's strategies and policies that directly contribute towards mitigating and adapting to climate change.

### Comprehensive Development

**5.3.** Proposals for development must conform to a comprehensive scheme for the whole site which relates to the wider Saltpans KIA as far as possible. Due to the policy designations that apply to the site it is likely that it will be developed in phases, with Phase 1 being the western part of the site (within the Key Industrial Area - KIA) and Phase 2 being the eastern part of the site (designated as Key Industrial Expansion Area - KIEA). The layout and orientation of development of Phase 1 should enable subsequent efficient development of Phase 2 and each phase will be expected to accord with the requirements of this Development Framework. However, both phases could come forward together where supported by evidence demonstrating that there are no suitable alternative sites available within any of the KIAs or Main Centres and Main Centre Outer Areas, enabling the KIEA to come forward.

### Development type

**5.4.** Given the constraints of the site, development will be limited to light industrial uses and/or general storage, which by their nature are acceptable in residential areas. These uses fall within Use Class 24 (light industrial) and Use Class 22 (general storage/distribution) of The Land Planning and Development (Use Classes) Ordinance, 2017. Depending on the details of any proposal, planning approval for such development is likely to include conditions relating to noise, dust and odour where



**Photo 10:** Example of small scale industrial units (North Harris). Photo credit: <https://www.north-harris.org/?tag=business-unit>



**Photo 11:** Example of small scale industrial units (Apollo Park, Banbury). Image credit: <http://www.apollopark.uk/>



**Photo 12:** Example of small scale industrial units (Cobo Business Park, Guernsey).

there is a risk of unacceptable nuisance arising from the development.

**5.5.** At the time of adoption of this Development Framework, the Committee for Economic Development has suggested that units for technology based activities and the digital sector, including those that would support Medical Technology (MedTech) and Financial Technology (FinTech) sectors, are likely to be in demand, along with small 'start-up' and 'scale-up' workshops and studios to support the entrepreneurial sector. Another area of potential demand relates to the processing of medicinal cannabis. However, the use classes proposed are broad and there is scope for a wide range of businesses which fall under the definitions of light industrial or general/storage and distribution.

### Access and Parking

**5.6.** The layout and design of the access must meet the requirements of Traffic and Highway Services and the access arrangements of Part P of the Guernsey Technical Standards. The vehicular access must be designed to enable vehicles to enter and egress safely, including emergency and refuse collection vehicles, to avoid conflict with pedestrian routes, and must take into account the constraints identified in the preceding analysis. Emergency access during a flood event should be taken into account and proposals must demonstrate that access is satisfactory in this regard.

**5.7.** At the current time, it is understood that vehicle access is unlikely to be achieved via the existing Braye Road Industrial Estate due to a lack of access rights, so the most likely scenario is that the 5m wide residential access road to the north, opposite Braye Road Garage, will be utilised to access the site (Access A). This is also the preferred access of the Fire Service to ensure direct access in an emergency.



**Photo 13:** Example of secure bike shed with sedum roof (by Langley Design Street Furniture). Photo credit: <https://www.externalworksindex.co.uk/entry/134476/Langley-Design-Street-Furniture/Sedum-Roof-Secure-Cycle-Shelters-for-Modern-Development/>



**Photo 14:** Example change in road texture and colour between residential and industrial access road. Photo credit: Coventry City Council Highway Design Guide



**Photo 15:** Example bollard lighting to minimise light pollution. Photo credit: <https://www.firstlighttechnologies.com/solar-lighting-applications/solar-pathway-lighting-2/>



**5.8.** If this access point is utilised, no vehicular through route to the rest of the industrial estate will be permitted in order to avoid the creation of a cut-through which may be used by drivers wishing to avoid the main Braye Road Industrial Estate junction. However, the layout of development must not prevent the creation of a vehicle access from the south should the opportunity arise in the future, at which point the vehicle access from the north would no longer be utilised (except for emergency vehicles if required). This is to ensure that the site can link to the wider Saltpans KIA in the future if appropriate.

**5.9.** Given the nature of the light industrial uses proposed on site there is not expected to be a significant requirement for HGV access, however HGV access would likely need to be severely restricted from this access point due to road safety and traffic management issues arising from HGVs turning.

**5.10.** If utilising this residential access, consideration could be given to a change in road surface (e.g. colour or texture) to differentiate the industrial access from the residential area to the north.

**5.11.** If access is to be achieved through the existing Braye Road Industrial Estate, via the Guernsey Press site, then the constraints identified in the Site Analysis section must be taken into account in designing access and egress routes. Early liaison with Traffic and Highways Services is recommended for all scenarios.

**5.12.** Safe access must be provided for pedestrians and cyclists to and within the site and measures included to encourage walking and cycling. A pedestrian footpath and cycling route linking Braye Road at the north of the site to the Guernsey Press building to the south of the site should be included. Any pedestrian or bicycle access over the douit must enable the current flow of the douit to remain.

**5.13.** Proposals must demonstrate that the road network can cope with the increased demand resulting from the new development from both a traffic management and road safety perspective. A Traffic Impact Assessment (TIA) will be required due to the potential level of intensification of use. This should include details of the traffic flows from the industrial estate onto Braye Road and, as necessary, information relating to the impact on traffic movements in the area of the Guernsey Press site. The full scope of the TIA will be determined with Traffic and Highways Services and will depend on the access option. Again, early discussion with Traffic and Highways Services is recommended in this regard.

**5.14.** In all cases, proposals for development must demonstrate that the access constraints and any traffic and highways concerns identified in the Traffic Impact Assessment can be satisfactorily overcome in order to obtain planning approval.

**5.15.** Appropriate levels of parking must be provided on site for cars, motorcycles and bicycles and include parking spaces for disabled people in accordance with adopted standards (see Parking Standards and Traffic Impact Assessments Supplementary Planning Guidance, 2016). Bicycle parking should be considered at the outset of the design process and be covered, secure, over-looked and easily accessible to encourage ease of use and modal shift. Electric vehicle charging points are expected to be provided as part of the development.

## Design and Layout

**5.16.** New development will be expected to achieve a high standard of design which respects the setting of the surrounding Protected Buildings and the relationship of the site to both residential and industrial uses. Development should be both accessible and sustainable and minimise negative impact on the environment through site layout and orientation, the use of low embodied energy materials, energy efficient buildings and incorporation of renewable energy technologies.

**5.17.** The southern aspect of the site presents opportunities to maximise passive solar design in order to minimise the use of energy to heat and cool the buildings. A planning application will need to include a statement that demonstrates how the buildings and associated spaces have been located to take advantage of climate and micro-climate so that energy use is minimised and that all renewable energy options have been considered and the proposed technology is the most appropriate for the site.

**5.18.** The effect of development on the setting of the Protected Buildings must be considered, as described in paragraph 19.6.7 of the IDP. In seeking to minimise harm to the setting of the Protected Buildings, no buildings will be permitted within 10 metres of the southern boundary of the Protected Buildings, and no buildings greater than 1 storey in height will be permitted within 15 metres of the southern boundary of the Protected Buildings.

**5.19.** The 'no building zone' could be used for surface car parking, landscaping or surface water management and will also contribute to the protection of residential amenity. This enables a transition in scale and massing of buildings between the domestic scale of Braye Road and the large scale industrial buildings of the Industrial Estate (such as Guernsey Press). Brightly coloured materials should be used with care, for example to highlight architectural detail, in order to respect the residential amenity and the setting of the Protected Buildings.



**Photo 16:** Example of public art (by Nerine Martini, Glenwood Community Hub, Blacktown Arts) Photo credit: <https://nerinemartini.com/> & <https://blacktownarts.com.au/tag/nerine-martini/>



**Photo 17:** Example of public art/seating. Credit: <https://www.davidgilvie.com/products/seats-benches>



**Photo 18:** Example of uncluttered entrance signage (The Avaries, Collings Road, Guernsey)



**5.20.** New buildings could be placed along the south and west boundaries of the site which can then enclose a landscaped courtyard (within which vehicles could park) that buildings face onto. This has the secondary benefit of the buildings being used to screen noise from within the courtyard.

**5.21.** The layout and design of the buildings should design-out opportunities for crime and anti-social behaviour - for example by passive surveillance and appropriate lighting - in accordance with Crime Prevention Through Environmental Design. Any need for security lighting will be balanced with the need to restrict light pollution and nuisance caused by easily triggered PIR (Passive Infrared) systems. The use of external lighting schemes should be minimised as far as possible, and be designed to consider wildlife impacts for example by using coloured, low intensity and directional options. Inappropriate boundary treatments such as chain link or steel palisade fencing will not be supported. Where enhanced security measures are required these should instead be incorporated into building design.

**5.22.** Locations for signage should be identified and designed in from the outset. Signage at the site entrance and within the site should be of a scale and appearance that is appropriate in the street-scene and is also clear and legible to visitors, contributing to the sites identity. Signage at the site entrance could form part of a public art scheme, and building façades should include an area within which signage for the building can be placed.

**5.23.** The site will be expected to include public art proposals of a scale that is proportionate to the site. This should be considered at the earliest possible stages of design to ensure that it appears and

functions as an integral component, rather than an add on feature. Consideration should be given to the Guernsey Arts Commissioning Guidelines. As noted, signage may be incorporated into a public art scheme. In enhancing the public realm, the scheme should consider the amenity of employers and employees, for example proposals could include outdoor seating. These elements can create a sense of place and identity for the development.

## Landscaping

**5.24.** A comprehensive landscaping scheme, informed by a site survey, must be submitted as part of any proposal. This scheme should respect local character and the setting of the Protected Buildings, mitigate against impacts of development such as the visual impacts of surface car parking, and enhance the biodiversity of the site. The landscaping scheme should support the goals and objectives of the 'Strategy for Nature Supplementary Planning Guidance 2020'. Notwithstanding the industrial use of the site, as far as possible, the excessive use of hard or artificial landscaping should be avoided and green spaces provided. Tree and shrub planting should be of predominantly native species. The trees, shrubs and vegetation along the eastern boundary must be retained.

**5.25.** The douit should be restored by removing concrete coverings and returning to open stream. Vertical douit edges should be graded back to a slope to create natural banks. A soft landscaped native buffer of 5m minimum adjacent to the douit should be included, where no built development or hard landscaping will be permitted. With water on site there are also opportunities to include other features such as a pond or other wetland habitat such as a small reedbed. This can provide a focal point, contribute to public realm and provide amenity to visitors and employees on site. The intention however should be to restore the douit as a wildlife corridor which ultimately connects with the Area of Biodiversity Importance to the east. Any proposed alterations to the douit will need Guernsey Water approval.

**5.26.** Landscaped buffers will be required along the northern and eastern boundaries of the site to soften the transition between domestic and industrial use and to compensate for loss of mature vegetation that has been recently removed along the western boundary. Opportunities to restore areas or pockets of grassland to species-rich semi-improved grassland habitat should be considered. The Planning Service can provide a standard planting list, but planting types may be restricted by the high water content and should be appropriate for the sites location within the Lowlands (Braye du Valle Wetlands) landscape character type.

**5.27.** It would be beneficial to include suitable bird and bat boxes and other biodiversity enhancements within the design of the development to allow bats to roost and birds such as Swifts, House Martins, House Sparrows and more common species to nest. The applicant/agent is encouraged to contact La Société Guernesiaise early in the design process in this regard.



**Photo 19:** Example of business park landscaping (Odyssey Business Park, West London). Photo credit: <https://www.thegardenco.co.uk/odyssey-business-park/>



**Photo 20:** Example of business park landscaping (Emerald Point Business Park). Photo credit: <http://www.valainc.com/portfolio/other/emerald-point-business-park/>



**Photo 21:** Example of footbridge over open stream. Photo credit: <https://stock.adobe.com/uk/>

**5.28.** A planning application must include a statement on what consultation has been carried out and what measures have been incorporated in the development that can improve the biodiversity of the site. Any planning approval is likely to include conditions to ensure the appropriate after-care and maintenance of wildlife areas on site.

## Contamination, Flood Risk and Water Management

**5.29.** Due to the risk of land contamination from previous uses on the site, any planning approval on site is likely to include conditions relating to the appropriate investigation and remediation of the land.

**5.30.** Planning applications for the site will be expected to demonstrate how the development has been designed to manage the risk of flooding, responding to both coastal flood risk and risk arising from potential surface water flooding.

**5.31.** As set out in paragraph 20.11.6 of the IDP, new developments in vulnerable areas are encouraged to contribute to the maintenance, improvement and development of coastal defences, and to develop sites and construct buildings in a way that alleviates the effects of flooding, thus securing protection for neighbours as well as the development site.

**5.32.** Further guidance on design to manage the risk of flooding can be provided by the Planning Service and Guernsey Water following a flood risk assessment. If the risk cannot be fully designed out (flood avoidance) then flood resistance and resilience measures will be required. Any application would be required to demonstrate how the development has been designed to manage the risk of flooding in reference to BS 85500:2015 Flood resistant and resilient construction.

**5.33.** A Surface Water Management Plan will be required as part of any planning application, detailing the treatment, attenuation and drainage measures proposed, e.g. collectors of water, permeable surfaces, attenuation areas, soakaways, indicative drainage runs. Surface water drainage must be dealt with on site and it is recommended that any paving is permeable and that sustainable drainage systems (SuDS) are incorporated to ensure that surface water run-off is properly controlled. SuDS proposals should be carried out in line with Guernsey Water's best practice guidelines and recommendations. The applicant/agent is encouraged to contact Guernsey Water early in the design process. SuDS proposals should be considered holistically with the design of the buildings, external spaces and landscapes. No surface water from the site shall enter the existing douit system and the douit system must be maintained.

**5.34.** SuDS opportunities to be explored include:

- The use of green or brown roofs, permeable paving in pedestrian areas and estate roads, and soft landscaping in line with Guernsey Water's best practice guidelines and recommendations;
- Effective control of pollution to the water environment in line with best practice set out in the CIRIA C758 SuDS manual.

## Utilities and Waste

**5.35.** Adequate utilities must be provided to serve the proposed development, including power and water supplies and foul water drainage services. Consultation should be undertaken with the relevant utility providers early in the development process. A planning application must include a statement that sets out what consultation has been undertaken with utility providers and how this has informed the design of development.

**5.36.** Guernsey Electricity have indicated that work is required on the substations on site. Guernsey Electricity will endeavour to accommodate future load growth relating to any successful future planning application.

**5.37.** Guernsey Water have indicated the need to carry out network modelling to better understand the impact of any development on the current foul water system in this area. The cost of such modelling would be borne by the developers. This should be undertaken early to front load the consideration of any detailed planning application proposals.

**5.38.** Guernsey Water have indicated that it may be necessary for large scale network upgrade of the potable water to feed future development in this area. It will be necessary to carry out network modelling to assess the effects of any new development. The costs of any network modelling exercises and any work required on the network to accommodate new development would be born by the developer. Again this should be undertaken early to front load the consideration of any detailed planning application proposals.

**5.39.** Guernsey Water also indicated there will be a requirement for break tanks to be considered as part of the design process to feed any proposed on-demand fire fighting system within new developments. A new fire hydrant will also be required.

**5.40.** Other ancillary structures or equipment should be incorporated into the initial building design and associated landscaping scheme.

## Refuse and Recycling

**5.41.** Designs must incorporate adequate individual or communal areas for the storage and collection of refuse and recyclable materials, taking into account the States of Guernsey waste strategy. The applicant/agent is encouraged to contact Guernsey Waste early in the design process in this regard (Telephone: 01481 231234 or email: recycle@gov.gg). Waste storage should form part of a comprehensive design for the landscape and external spaces.



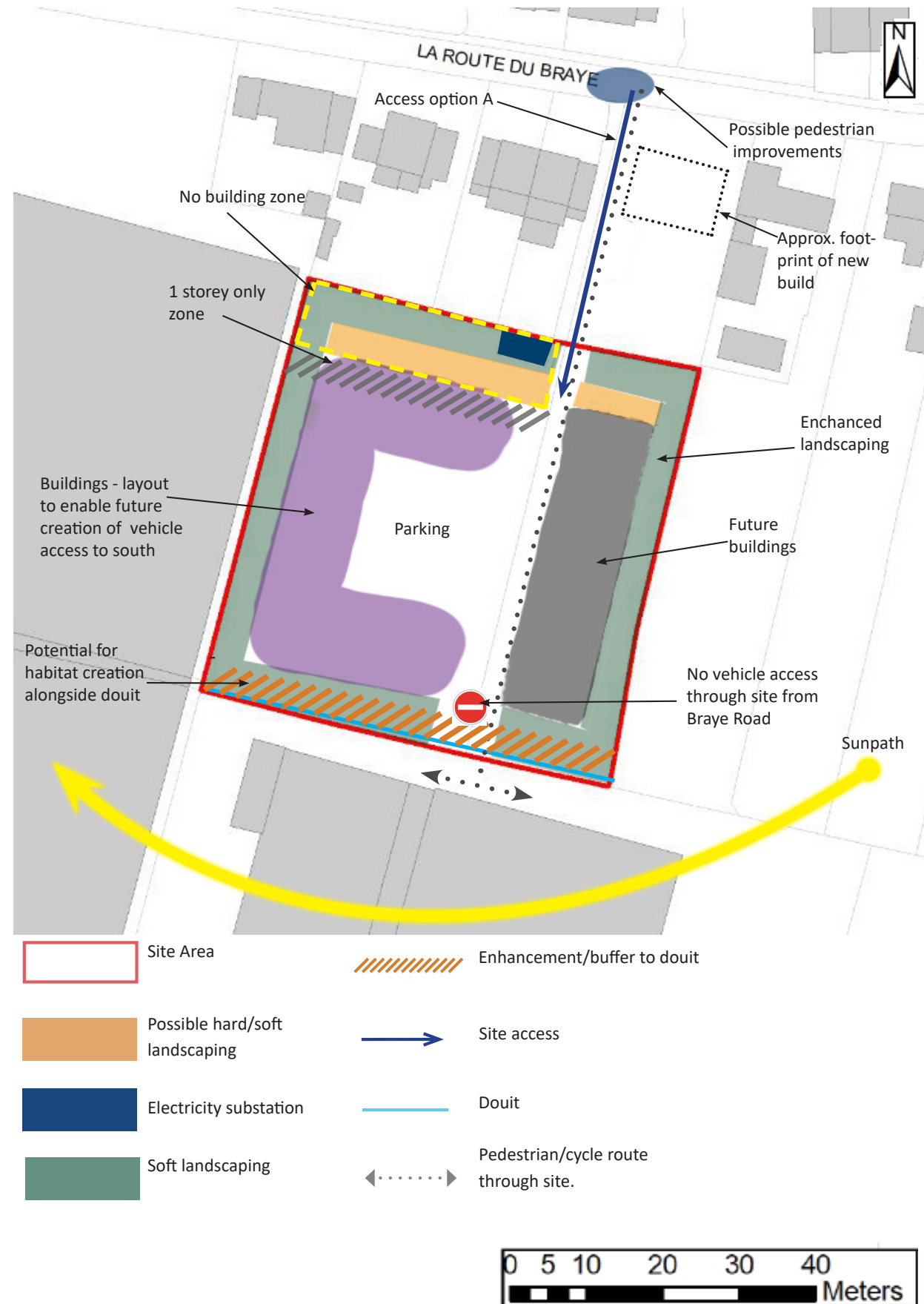


Image 8: Development Guidelines

## 6. Construction Waste and Environment

**6.1.** The former horticultural use of the site could give rise to potentially contaminated land. A survey may be required to establish if remediation is required.

**6.2.** A particular sensitivity in this location is the douit running along the southern boundary of the site. Guernsey Water should be consulted prior to works starting to ensure suitable protection measures are in place to prevent contamination or drainage issues. The flow of water must be maintained both during and after construction.

**6.3.** Depending on the floor area proposed, a detailed Site Waste Management Plan may be required at planning application stage. Further guidance can be found in the Site Waste Management Plans Planning Advice Note which can be viewed here: <https://www.gov.gg/CHttpHandler.ashx?id=113558&p=0>

**6.4.** Site Waste Management Plans apply to all aspects of a project, with the majority of opportunities for waste minimisation existing at the design phase. Information should be provided with a planning application on the amount and type of waste that will be produced during the course of a project and how waste will be reduced, reused, recycled, recovered or disposed of. This should be a living document, drafted from the conception of a project and being added to and evaluated until the completion of the development and submitted again to the Authority prior to the use of any building on the site.

**6.5.** The final design will incorporate dedicated waste and recycling storage provision on the site suitable to the density of the development approved as set out in the Development Guidelines.

**6.6.** A Construction Environmental Management Plan (CEMP) will be required if there is 1000sqm or more industrial floorspace created. Consideration of this will be given at the planning application stage. Information should be provided at the planning application stage detailing how construction traffic would access the site and be managed during the construction phase.

**6.7.** An Environmental Impact Assessment (EIA) screening opinion for the site is not required as the area of development is less than 1 hectare. Furthermore, whilst the site is related to the existing Braye Road Industrial Estate and the wider Saltpans KIA, no significant adverse effects on the environment are envisaged from the development of this site that would trigger the requirement for an EIA screening opinion as set out in Schedule 2(j)(ii) of The Land Planning and Development (Environmental Impact Assessment) Ordinance, 2007.



## Appendix 1: IDP Policy Context

IDP Policy	Policy Relevance
<b>S1: Spatial Policy</b>	The spatial policy in the IDP sets out that the Main Centres and Main Centre Outer Areas have the role as the focal point for development in the Island to maintain the vitality of these areas.
<b>S2: Main Centres and Main Centre Outer Areas</b>	The Main Centres provide the core focus for development within the Island and proposals for development in these areas will generally be supported. Proposals for development within the Main Centre Outer Areas will also generally be supported where this would not detract from the objective of ensuring the Main Centres remain the core focus for economic and social growth. In both cases proposals must meet the requirements of the relevant specific policies of the Island Development Plan.
<b>MC5(A): Industry, Storage and Distribution Uses in Main Centres and Main Centre Outer Areas - within Key Industrial Areas and Key Industrial Expansion Areas</b>	<p>The main focus of industry remain within and around the Main Centres, specifically including Admiral Park and Saltpans. The IDP (2016) seeks to consolidate industry and storage and distribution uses on designated Key Industrial Areas together with identified opportunities for expansion of these areas, if needed in the future, to allow further clustering of industrial type uses and enhancement in the quality of accommodation to meet modern needs. The designation is flexible enough to enable a range of uses and industrial sectors.</p> <p>The re-use and redevelopment of existing sites is required before proposals for the development of the identified expansion areas in order to consolidate activity and economic investment in the KIAs and achieve effective and efficient use of land. Development of Key Industrial Expansion Areas will only be supported where it has been demonstrated that no suitable alternative sites are available within any of the Key Industrial Areas or Main Centres and Main Centre Outer Areas.</p> <p>Development Frameworks are required for Key Industrial Expansion Areas, as well as the undeveloped part of the Saltpans KIA.</p>
<b>GP1: Landscape Character and Open Land</b>	This policy sets out that development will be supported where it respects the relevant landscape character type within which it is set, where development does not result in the unacceptable loss of any specific distinctive features that contribute to the wider landscape character and local distinctiveness of the area, and takes advantage where practicable of opportunities to improve visual and physical access to open and undeveloped land. A landscaping scheme will be required for this scale of development.

<b>GP5: Protected Buildings</b>	The particular special interest of the surrounding Protected Buildings, including their special characters, features and setting, must be considered so that, as far as possible, that special character is not adversely affected and is preserved in a manner which is proportionate to its special interest.
<b>GP8: Design</b>	<p>Development, including the design of necessary infrastructure and facilities, is expected to achieve a high standard of design which respects, and where appropriate, enhances the character of the environment.</p> <p>Development must respect the character of the local built environment and provide soft and hard landscaping to reinforce local character and/or mitigate the impacts of development including contributing to more sustainable construction. The amenity of occupiers and neighbours is also important- see IDP Annex I for further information.</p> <p>Development should also provide adequate areas for storage of refuse and recyclable materials.</p>
<b>GP9: Sustainable Development</b>	<p>This policy is wide ranging and includes requirements for sustainable design and construction with reference to the design, layout and orientation of buildings and surface water drainage, renewable energy and use of materials. Hard landscaping should include the use of permeable paving and other Sustainable Drainage Systems (SuDS). The design of soft landscaping can also help in accordance with this policy.</p> <p>Development of 1,000 square metres of floor area will require a Site Waste Management Plan.</p> <p>The Site Waste Management Plans Planning Advice note can be found here- <a href="http://www.gov.gg/planning_building_permissions">www.gov.gg/planning_building_permissions</a>.</p>
<b>GP10: Comprehensive Development</b>	Individual proposals must conform to a comprehensive scheme for the whole site or area in order to make the most effective and efficient use of land.
<b>GP18: Public Realm and Public Art</b>	Any proposal should consider the relationship between the development and the public realm and it is expected to enhance where possible. Public art can take the form of an integral part of a development or as a standalone feature.

<b>IP1: Renewable Energy Production</b>	Proposals for renewable energy installations (and ancillary and associated development) will be supported, where they can be satisfactorily incorporated into the built form of the proposed development.
<b>IP6: Transport Infrastructure and Support Facilities</b>	Development proposals that encourage a range of travel options to and within the Main Centres and the Main Centre Outer Areas will be supported, where they are compatible with other relevant policies of the Island Development Plan. The Authority will require development to be well integrated with its surroundings. Pedestrian and bicycle access within the site must be incorporated to take opportunities to increase connectivity and create links and public walkways where appropriate.
<b>IP7: Private and Communal Car Parking</b>	<p>The parking standards for the IDP are set out in the Supplementary Planning Guidance: Parking Standards and Traffic Impact Assessment. For residential development in the Main Centre Outer Areas the car parking standards are in section 6. The standards are maximums and the development would be expected to be within these standards.</p> <p>Provision will need to be made for secure covered bicycle parking for residents and visitors.</p>
<b>IP9: Highway Safety, Accessibility and Capacity</b>	The public road network's ability to cope with increased demand, physical alterations required to the highway, and the access requirements of all people will be considered.
<b>IP10: Coastal Defences</b>	Paragraph 20.11.6 of the IDP sets out that new developments in vulnerable areas are encouraged to contribute to the maintenance, improvement and development of coastal defences, and to develop sites and construct buildings in a way that alleviates the effects of flooding, thus securing protection for neighbours as well as the development site.
<b>IP11: Small Scale Infrastructure Provision</b>	Proposals for small scale infrastructure provision will be supported where this would contribute to the maintenance and support of efficient and sustainable infrastructure, the applicant being required to demonstrate that the sharing or co-location of facilities, buildings, apparatus and support structures is not practically possible.

**Contact Us** for further information and advice at: Planning Service, Sir Charles Frossard House, St Peter Port. GY1 1FH Telephone 01481 226200 Email [planning@gov.gg](mailto:planning@gov.gg)

**Have you visited our website?** Go to [www.gov.gg/planningandbuilding](http://www.gov.gg/planningandbuilding) for additional guidance material and other planning information, including how to request pre-application advice.

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